

Study Area C

Recommendations and Principal Reasons

Study Area C is a large area of northwestern Miami-Dade County bounded by the Miami Canal (C-6) on the northeast, the Florida East Coast Railroad Canal on the east, Tamiami Canal (C-4) on the south, and Levee 30 on the west. Most of this study area is unincorporated except for the new city of Doral and relatively small portions in the cities of Medley and Sweetwater.

Two applications, Applications No. 6 and 7, were filed requesting amendments to the Land Use Plan map in this area. Both applications are requesting land use changes and the extension of the Urban Development Boundary (UDB) to include the property.

Application Number	Applicant/Representative Location (Size) REQUESTED CHANGES TO THE LAND USE PLAN MAP	Recommendations for •TRANSMIT •DISPOSITION
6	Doral West Commerce Park, LLC / Felix M. Lasarte, Esq. West of the Turnpike (HEFT) and east of NW 122 Avenue at approximately NW 22 Street (2.5 Gross Acres) From: Open Land To: Restricted Industrial and Office and Include within the Urban Development Boundary Standard Amendment	DENY TRANSMIT
7	LOWE'S HOME CENTERS, INC. / Juan J. Mayol, Jr., Esq. and Richard A. Perez, Esq. Northwest corner of Theoretical SW 138 Avenue and north of the Tamiami Canal (SW 8 Street) (21.6 Gross Acres) From: Open Land To: Business And Office and Include within the Urban Development Boundary Standard Amendment	DENY TRANSMIT

Application No. 6

Location: West of the Turnpike (HEFT) and east of NW 122 Avenue at approximately NW 22 Street (2.5 Gross Acres)

Requested Amendment to Land Use Plan Map:

From: Open Land

To: Restricted Industrial and Office and
Include within the Urban Development Boundary

Recommendation: DENY and TRANSMIT

Principal Reasons for Recommendations:

1. Policy 8G of the Land Use Element provides guidance regarding the need to move the 2005 Urban Development Boundary (UDB) for non-residential land uses. The UDB is included on the Land Use Plan (LUP) map to distinguish the area where urban development may occur through the year 2005 from areas where it should not occur. The policy states the following: “The adequacy of non-residential land supplies shall be determined on the basis of land supplies in subareas of the County appropriate to the type of use, as well as the Countywide supply within the UDB. The adequacy of land supplies for neighborhood- and community-oriented business and office uses shall be determined on the basis of localized subarea geography such as Census Tracts, Minor Statistical Areas (MSAs) and combinations thereof. Tiers, Half-Tiers and combinations thereof shall be considered along with the Countywide supply when evaluating the adequacy of land supplies for regional commercial and industrial activities.” Countywide, the supply of land for industrial use will not be depleted until after 2025.

To amend the year 2005 Urban Development Boundary at this time to enable expansion of industrial development would be premature. At the beginning of 2004, the existing supply of vacant industrial land in Study Area C (MSA 3.2) consisted of 1,999.4 acres. The absorption of such land over the 2003 to 2025 period is projected at an average annual rate of 68.71 acres. Based on the projected rate of absorption reflecting the past rate of such uses, the existing supply of industrial zoned land in the study area would last until the year 2022. These dates are sufficiently beyond the time horizons of the current CDMP and associated County service plans to warrant changing the currently adopted development boundary at this time.

2. Policy 8H of the Land Use Element states that when considering land areas to add to the UDB, some areas such as “Future Wetlands delineated in the Conservation and Land Use Element” should be avoided. According to the most updated map of future wetlands, the application site is on the edge of these wetlands.
3. This proposal would be spot planning. The Department believes that the issue of filling in the hole in the donut created by Beacon Lakes DRI CDMP Amendment should be addressed in a comprehensive, rather than in an incremental manner.
4. The application site has no access to an existing roadway.
5. Traffic counts currently do not exist in this area for roadways west of the Homestead Extension of the Florida Turnpike. However, trips were distributed to roads located to the east. The segment of NW 25 Street between NW 97 and 107 Avenue is currently failing without the application with a Level-of-Service (LOS) of “F.” The LOS is represented by one of the letters “A” through “F”, with “A” generally representing the most favorable driving conditions and “F” representing the least favorable. The adopted LOS standard on these roadways is “D.”
6. The subject property does not impact any historical resources but does impact environmental resources with a location in the North Trail Basin, Transitional Northeast

Everglades and the Northwest wellfield protection area. The development criteria for water management and flood protection in the North Trail Basin requires the set aside of areas to be converted into lakes (28.6 % of the net acreage) or dry retention area (33 % of the net acreage). The Transitional Northeast Everglades contains jurisdictional wetlands, which requires a Miami-Dade Class IV Wetlands Permit.

According to Chapter 24 of the County Code, any non-residential use which generates, uses, handles, disposes of, discharges or stores hazardous wastes is prohibited in the wellfield protection area. These regulations further prohibit the use of septic tanks unless it is for residential purposes or uses ancillary to rock mining operations. These prohibitions are reinforced in the Development Order for the Beacon Lakes Development of Regional Impact, a project adjacent to the application site.

7. While the Department does not believe that the adopted 2005 and 2015 LUP map of the CDMP should be amended and the 2005 UDB be extended to provide for additional industrial land at this location, transmittal is recommended to provide the application an opportunity to undergo consideration through the full plan amendment review process.

Application No. 7

Location: Northwest corner of Theoretical SW 138 Avenue and north of the Tamiami Canal (SW 8 Street) (21.6 Gross Acres)

Requested Amendment to the Land Use Plan Map:

From: Open Land

To: Business and Office and
Include within the Urban Development Boundary

Recommendations: DENY and TRANSMIT

Principal Reasons for Recommendations:

1. The applicant has provided no new information since the application site was denied on November 5, 2003 by the Board of County Commissioners that would support the request. This application was Parcel A of Application No. 10 in the April 2003 Cycle. Parcel B of that application, which was approved on May 5, 2004 by the Board of County Commissioners, consisted of 16 acres that was redesignated on the adopted 2005 and 2015 Land Use Plan (LUP) map from "Industrial and Office" to "Business and Office" for a proposed Lowe's Home Center.
2. Policy 8G of the Land Use Element provides guidance regarding the need to move the 2005 Urban Development Boundary (UDB) for non-residential land uses. The UDB is included on the LUP map to distinguish the area where urban development may occur through the year 2005 from areas where it should not occur. The policy states the following: "The adequacy of non-residential land supplies shall be determined on the basis of land supplies in subareas of the County appropriate to the type of use, as well as the Countywide supply within the UDB. The adequacy of land supplies for

neighborhood- and community-oriented business and office uses shall be determined on the basis of localized subarea geography such as Census Tracts, Minor Statistical Areas (MSAs) and combinations thereof. Tiers, Half-Tiers and combinations thereof shall be considered along with the Countywide supply when evaluating the adequacy of land supplies for regional commercial and industrial activities.” Countywide, the supply of land for commercial and office developments will not be depleted until 2025.

To amend the year 2005 Urban Development Boundary at this time to enable expansion of urban commercial development would be premature for a site located in the North Central Tier and in Study Area C. The North Central Tier has a 21-year supply of vacant commercial land at the current absorption rate.

Study Area C (MSA 3.2) contained 429.3 acres of vacant land zoned or designated for commercial uses in the year 2004. The average annual absorption rate projected for the 2003-2025 period is 17.2 acres per year. At the projected rate of absorption, the study area will deplete its supply of commercial zoned or designated land beyond the year 2025. In addition, its commercial acres per thousand persons ratio exceeds the County average for both 2015 and 2025. These dates are sufficiently beyond the time horizons of the current CDMP and associated County service plans to warrant changing the currently adopted development boundary at this time.

3. Policy 8H of the Land Use Element states that when considering land areas to add to the UDB, some areas such as “Future Wetlands delineated in the Conservation and Land Use Element” should be avoided. According to the most updated map of future wetlands, the application site is on the edge of these wetlands.
4. This property is located outside the 2005 Urban Development Boundary and west of the intersection of two major roadways, SW 137 Avenue and SW 8 Street or Tamiami Trail. Guidelines of Urban Form in the CDMP state “Intersections of section line roads shall serve as focal points of activity, hereafter referred to as activity nodes. Activity nodes shall be occupied by any nonresidential components of the neighborhood including public and semi-public uses. When commercial uses are warranted, they should be located within these activity nodes.” The intersection of SW 8 Street and SW 137 Avenue does qualify as a location for an activity node.

The maximum size of these nodes is typically 40 acres to serve the surrounding neighborhoods. Currently, on the south side of the intersection of SW 8 Street and SW 137 Avenue, there is a total of 29.7 acres of developed or developing commercial and office properties. The area to the south currently has a Publix Supermarket, two pharmacies (Walgreens and Eckerd), a bank, two gas stations and numerous stores in several strip shopping centers between the Felix Varela Post Office at 14310 SW 8 Street and SW 199 Avenue, and a neighborhood shopping center, Tamiami Shops, in the SW quadrant of SW 137 Avenue and SW 8 Avenue. In addition, there is a 16-acre vacant commercial site on the north side of intersection as a result of application No. 10 from the April 2003 CDMP amendment cycle. Thus, over 40 acres in the vicinity of the application site are already available for commercial uses in this area.

5. The reason for this application is to expand the existing 16-acre site for a Lowe's Home Center by adding 21.6 gross acres of land that is located outside the UDB. The existing 16 acres should be of a sufficient size to support a building supply business. The one existing Lowe's Home Center in Miami-Dade County is located on a 13.3-acre site at 17460 NW 57 Avenue with an approximately 158,000 sq. ft. structure.

Since Lowe's is relatively new to Miami-Dade County, a sample of four Home Depot locations in Miami-Dade County was reviewed in 2003 to determine the acreage usually needed for this type of business. The largest of these properties was a 15-acre parcel at 33001 Dixie Highway in Florida City with a 130,000 sq. ft structure built in 2000. The smallest Home Depot site in the sample at 1397 SE 10 Court in Hialeah was replaced in 2004 with a new 135,000 sq. ft. store at 950 SE 12 Street in the same city on two parcels with a total acreage of 10.99.

6. The subject property does not impact any historical resources but does impact environmental resources with a location in the North Trail Basin, North Trail Wetland Basin and the West wellfield protection area. The development criteria for water management and flood protection in the North Trail Basin requires the set aside of areas to be converted into lakes (28.6 % of the net acreage) or dry retention area (33 % of the net acreage). The North Trail Wetland Basin contains jurisdictional wetland, which requires a Miami-Dade Class IV Wetlands Permit and compliance with the North Trail/Bird drive Everglades Basin Ordinance, including plans for mitigation, tree island preservation and fill encroachment/stormwater management criteria will be required before any work can take place on the property. According to Chapter 24 of the County Code, any non-residential use which generates, uses, handles, disposes of, discharges or stores hazardous wastes is prohibited in the wellfield protection area. The regulations further prohibit the use of septic tanks unless it is for residential purposes or uses ancillary to rock mining operations.
7. The impact to public services is limited. This application will require a new sewer pump station. The Miami-Dade Fire Rescue Department has stated that the impact to existing fire rescue services is severe since the average travel time for these services is approximately 8.2 minutes and the site is located outside the service area of a fire station. However, a new fire rescue station is programmed for fiscal year 2008 that will serve the site.
8. While the Department does not believe that the Adopted 2005 and 2015 LUP map of the CDMP should be amended and the 2005 UDB be extended to provide additional land for a shopping center at this location, transmittal is recommended to provide the application an opportunity to undergo consideration through the full plan amendment review process.

Study Area C Description

Study Area C includes a large area of approximately 104 square miles located in west central Miami-Dade County. This study area is bounded by the Miami Canal (C-6) on the northeast, the Florida East Coast Canal on the east, Tamiami Canal (C-4) to the south, and Levee-30 on the west.

This study area is largely unincorporated except for three cities, Medley, Sweetwater and the newly formed City of Doral, located east of the Florida Turnpike. This study area consists of large one minor statistical area (MSA 3.2) for which population and land use data are regularly maintained. These boundaries include sufficient area to reasonably represent the trend of residential, commercial and industrial development in the vicinity of the applications discussed below. (See Figure C-1.)

Environmental Conditions and Considerations

Study Area C is a low-lying area of former Everglades. Land elevations are generally four to six feet above mean sea level (msl). During most years the water table is less than one foot below the natural land elevation for four to six months. Very poorly drained muck soils and fine sands predominate in the west and southern parts of the area. This soil has severe limitations for development because of its wetness and shallowness to bedrock; however, local construction methods are usually able to overcome these problems.

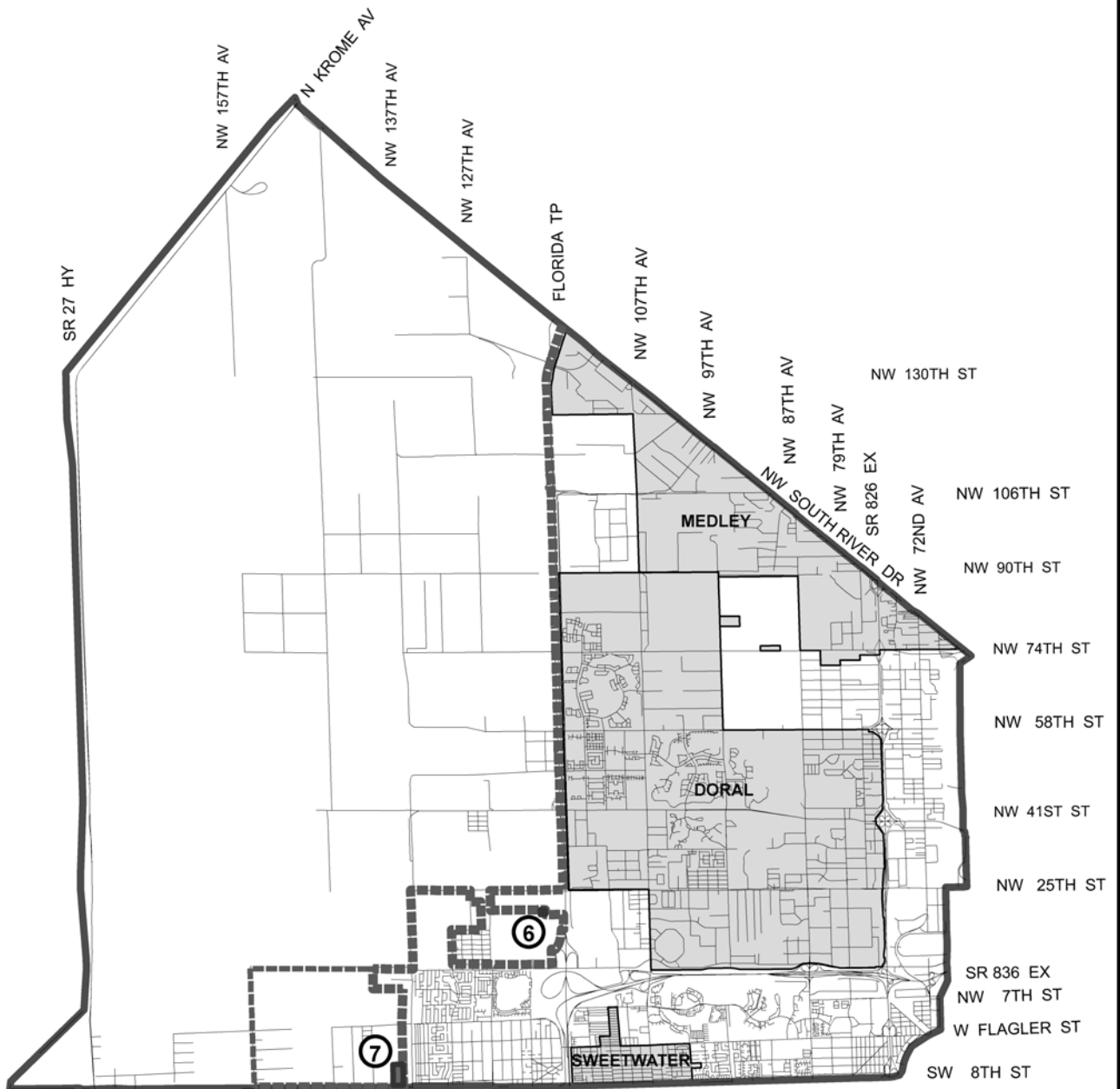
A summary of the environmental conditions for Study Area is presented in Table C-1 below.

Table C-1
Environmental Conditions
Study Area C

	Application No.	
	6	7
<u>Flood Protection</u>		
County Flood Criteria (NGVD)	+7.5 feet	+ 8 feet
Drainage Basin	North Trail	North Trail
Stormwater Management Requirements	5-year storm, on site	5-year storm, on site
Federal Flood Zone	Zone AH	Zone AH
Hurricane Evacuation Zone	NONE	NONE
<u>Biological Conditions</u>		
Wetlands Permits Required	YES	YES
Native Wetland Communities	YES	YES
Natural Forest Communities	YES	YES
Endangered Species Habitat	NO	NO
<u>Other Considerations</u>		
Within Wellfield Protection Area	YES	YES
Archaeological/Historical Resources	NO	NO
Within Area of Known Contamination	NO	NO

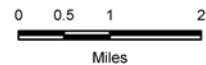
Sources: Miami-Dade County Departments of Environmental Resources Management, Planning and Zoning, and Historic Preservation, 2005

Figure C-1
LOCATION: Study Area C (MSA 3.2)



Legend

- 2005 Urban Development Boundary
- 2015 Urban Expansion Area
- ▬ Study Area
- ▬ Municipality
- ⑦ Application Area



SOURCE: MIAMI-DADE COUNTY, DEPARTMENT
 OF PLANNING AND ZONING, 2005



Flood Protection

Application Nos. 6 and 7 are located in the North Trail Basin, where development criteria for water management and flood protection shall be complied with through set aside of areas to be converted into lakes or dry retention areas capable of retaining a 5-year storm on site. Neither of the application sites is located in a Hurricane Evacuation Area, however, both sites are located within Federal Flood Zone AH and may flood during a 100-year storm. All applicants must develop their property based on Chapter 11C of the Miami-Dade County Code and comply with federal, State, and County regulations concerning flood-prone areas.

Wetlands

Application Nos. 6 and 7 are located in the Transitional Northeast Everglades and in the North Trail Wetland Basin, which is a jurisdictional wetland area according to the Environmental Protection Ordinance of Miami-Dade County. Therefore, the applicants must obtain Class IV Wetland Permits and comply with the North Trail/Bird Drive Everglades Basin Ordinance. Additionally, plans for mitigation, tree island preservation and fill encroachment/stormwater management criteria will be required before any work can take place on the subject properties. These criteria require on-site stormwater management and a mitigation contribution to fund off-site mitigation to compensate for wetland values lost as a result of the proposed projects. Additional permits from the US Army Corps of Engineers (ACOE), State of Florida Department of Environmental Protection (DEP), and the South Florida Water Management District (SFWMD) may be required for the proposed projects. It is the responsibility of the applicants to contact these agencies.

Forest Resources

Application Nos. 6 and 7 contain tree resources; however the properties are located in a designated wetland basin and will be regulated through a Class IV Wetland Permit.

Wellfield Protection

Application Nos. 6 and 7 are located within the boundaries of the Northwest and West Wellfield Protection Areas, respectively. According to Miami-Dade County Code, no hazardous materials can be used, generated, handled, disposed of, discharged or stored within these western protection areas. These prohibitions are reinforced in the Development Order for the Beacon Lakes Development of Regional Impact, a project adjacent to the Application No. 6 site.

Historical and Archeological

No archaeological or historic resources have been identified on the Application sites; therefore Application Nos. 6 and 7 have a low probability for the presence of archaeological and historical resources; however, any ground disturbing activities should be monitored by the County Archaeologist.

Land Use Patterns Within Study Area C

The character of the area varies greatly from the industrial and office uses that dominate the large northeastern portion of the area, to the concentrations of residential development that dominate the southern Fontainebleau Park-Sweetwater areas south of the Dolphin Expressway. The central Doral area has also been undergoing a rapid increase in residential development in recent years. Housing types in the study area include a full range from single-family detached dwellings at estate density to multifamily dwelling units at medium density. Significant business developments also exist in the area. Notable areas include the Miami International Mall at NW 12 Street and NW 107 Avenue, Mall of the Americas (formerly Midway Mall) at Flagler Street and the Palmetto Expressway and Flagler Park Plaza immediately to its west, Beacon Center at NW 87 Avenue and NW 12 Street, and Dolphin Mall at NW 12 Street and the Turnpike. Of the approximately 41 square miles inside the Urban Development Boundary (UDB) in this study area, approximately 5,000 acres or just under 20 per cent remains undeveloped.

The western 60 per cent of this Study Area lies outside of the UDB. The UDB runs along the alignment of the Florida Turnpike Extension through most of the area, and turns west around NW 25 Street to the general vicinity of NW 137 Avenue where it turns southward. As mentioned in the previous section, much of the western area is floodplain, wetland, and water recharge area. The Northwest Wellfield, the source of potable water for most of northern Miami-Dade County, is located in the east-central portion of the non-urbanized area, one and one-half miles west of the Turnpike. The principal land use outside the UDB is limestone mining. The Study Area also contains some major public facilities and institutional uses, parks and recreation land, and some agricultural activities.

Future Land Use Patterns. The future development pattern adopted for this study area provides primarily for continued development of offices and industrial uses in the eastern portion of the area inside the UDB. This area has been planned for this activity to provide for economic activity associated with Miami International Airport, and to prevent incompatible uses from occupying land under the flight paths. This area is both close to the airport freight area, is served by several expressways including the Palmetto, Dolphin, and Turnpike, has good access to both rail and truck terminals, and much of the area is directly affected by the heavy air traffic. Land around major utility and other transportation facilities is also planned for industrial activity. Residential areas north of the Dolphin Expressway (SR 836) are planned to concentrate in the area west and northwest of Doral Park away from the airport, and south of SR 836 in the Fontainebleau Park-Sweetwater corridor.

The LUP map was amended in March 1997 to designate a 191-acre parcel of land from "Industrial and Office" to "Business and Office" to allow development of the Dolphin regional mall at the northeast corner of the Turnpike and NW 12 Street. This was done in conjunction with approval of the Beacon TradePort Development of Regional Impact (DRI). The DRI will contain 183-acres of industrial warehousing uses and 191-acres of retail mall. A Turnpike interchange was constructed at NW 12 Street in association with that DRI.

The UDB was moved northward in 2002 to include two large industrial and office projects, Beacon Lakes and Shoppylane, that are located south of NW 25 Street and east of NW 137

Avenue. These two projects added in 2002 a total of 571.45 acres of land to the supply of industrial land in the Study Area.

During the last major update of the CDMP, several sections of land west and northwest of Doral were redesignated to residential use to provide increased opportunities for employees to live in proximity to their jobs. In October 1996, at the conclusion of the November 1995-96 CDMP amendment cycle, the Board of County Commissioners redesignated 560 acres in the section of land located between NW 74 and 90 Streets from NW 107 to 117 Avenues, from "Industrial and Office" to "Low Density Residential/One Density Increase With Urban Design" to expand residential opportunities in this dominant employment center. Land so designated is eligible for approval of development at up to 13 du/ac if sound urban design principles are utilized.

The extensive area west of the Turnpike Extension north of NW 25 Street is designated as "Open Land" to protect the natural resources in the area for their continued productive use. Notably, important water resources and construction minerals occur in that area which warrant conservation and sound management to enable their continued benefits to accrue to the public. The area lies outside of the area provided water table control by the Central and South Florida Flood Control Project and, as such, was labeled "Area B" by the Corps of Engineers, as contrasted with flood-protected "Area A" to the east. Thus far, CDMP policy has successfully prevented urban development, thereby preventing flood damage to structures in this flood-prone area, preventing demands for drainage of water resources to tide, protecting the Northwest Wellfield, and maintaining the availability of the important construction mineral deposits in the area.

The Open Land designation of this area originated with the initial adoption of the CDMP in 1975 following the severe drought of 1970 through 1971, adoption of the County's Water Quality Master Plan in 1973 which led to the consolidation of the City of Miami and Dade County water and sewer utilities into the Miami-Dade Water and Sewer Authority (now County Department), and the completion of the first "East Everglades" environmental planning studies in 1974, which included this interior wetland area of the County. The CDMP was subsequently refined after adoption of the Northwest Wellfield Protection Plan by the Board of County Commissioners in late 1985 (R-1541-85). Most of the area was also included in a recent "Lake Belt" planning area study initiated by the Florida Legislature to help maintain the availability of construction minerals for the State and to help protect and develop water resources and recreational opportunities in the area. It is currently part of a large area being studied by the U.S. Army Corps of Engineers and the South Florida Water Management District for possible development of water storage and treatment areas as Everglades buffer areas.

A summary of existing land uses adjacent to the application sites is presented in Table C-2.

Table C-2
Existing Land Uses Within and Adjacent to Application Area
Study Area C-2

Application No.	Application Area	Adjacent to Application Area on the:			
		North	East	South	West
6	Agriculture	Beacon Lakes industrial park (IU-1)	Beacon Lakes industrial park (IU-1)	Agriculture (GU)	Agriculture (GU)
7	Vacant (GU)	Vacant (GU)	Vacant (GU / IU-C)	Canal, Tamiami Trail, Strip Shopping Center (BU-1A)	Vacant (GU)

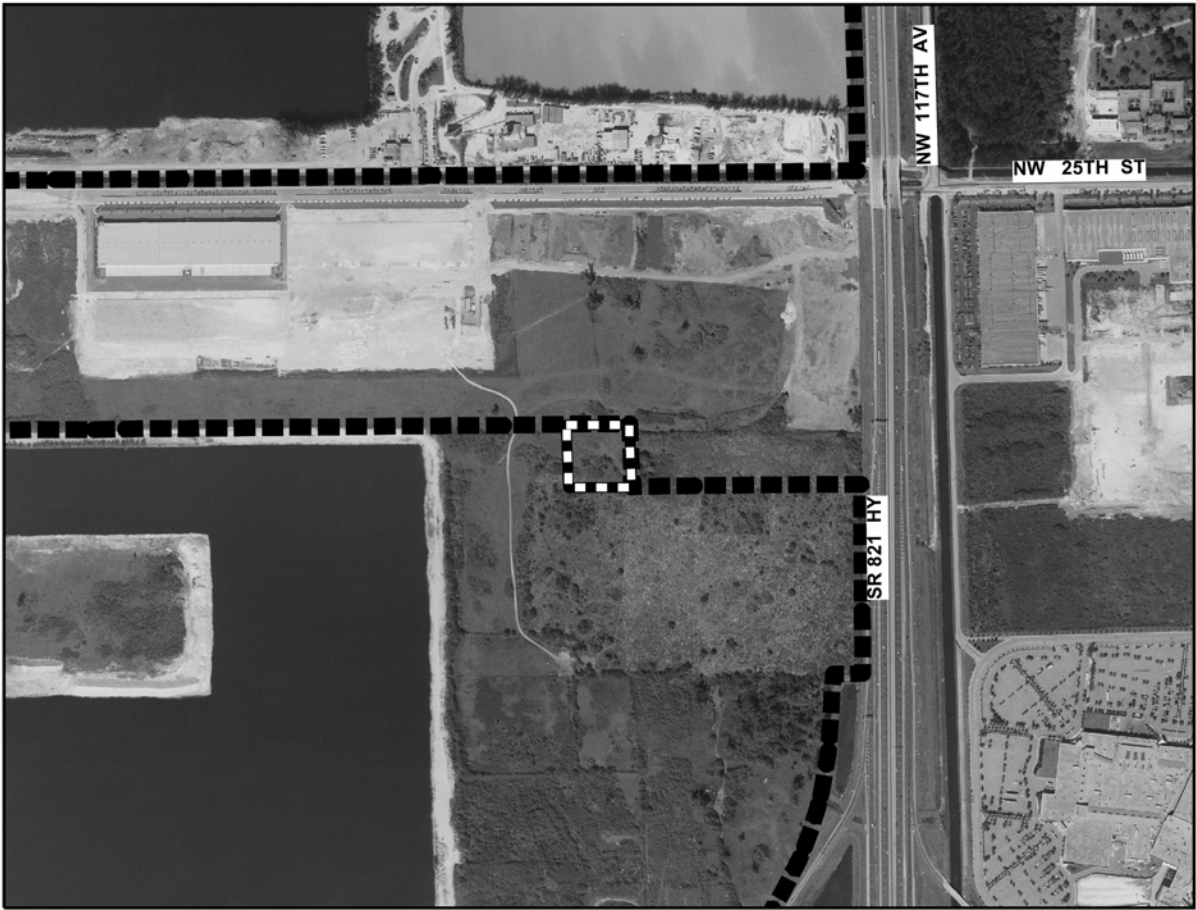
Application No. 6

This small-scale application site contains a total of approximately 2.5 acres. The application site lies outside the 2005 Urban Development Boundary (UDB) and is bounded on the south by Theoretical SW 20 Street, on the east by Theoretical SW 119 Avenue (along the UDB boundary), on the north by Theoretical SW 21 Street (along the UDB boundary), and on the west by Theoretical SW 120 Avenue. The applicant is requesting that the land use of the application site be re-designated from “Open Land” to “Restricted Industrial and Office” and that the UDB be moved to the western and southern boundaries of the parcel.

Existing Land Use Pattern. Current zoning and the development pattern promoted by the CDMP Land Use Plan map are depicted in Figures C-2 through C-4. The application site currently consists of agricultural property. Lands to the immediate south and west of the application site are agricultural, with former agricultural areas immediately to the north and east transitioning to Light Industrial and Business uses as part of the Beacon Lakes Development of Regional Impact. Located further east and southeast of the site are the Florida Turnpike (HEFT), NW 117 Avenue, and Dolphin Mall. Further north and west of the site lie rock mining operations and excavated lakes. These lands are zoned GU – Interim use. Located further south and southwest of the site are SR 836 (Dolphin Expressway) and a large multi-family and single-family residential area.

Future Development Pattern. The site and lands to the south and west (outside the UDB) are currently designated as “Open Land” (GU) on the Land Use Plan map (LUP). Immediately to the north and east of the site (within the UDB), the land use is designated as “Industrial and Office” (IU-1), and will continue to develop as a light industrial park as part of the Beacon lakes DRI. Further east, beyond NW 117 Avenue, the land use is designated as “Industrial and Office”, with a “Business and Office” designation to the southeast. The portion of the CDMP Land Use Plan map that depicts the area surrounding this application site is included as Figure C-5.

Figure C-2
AERIAL PHOTO: APPLICATION NO. 6



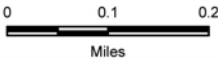
2005 AERIAL



APPLICATION AREA



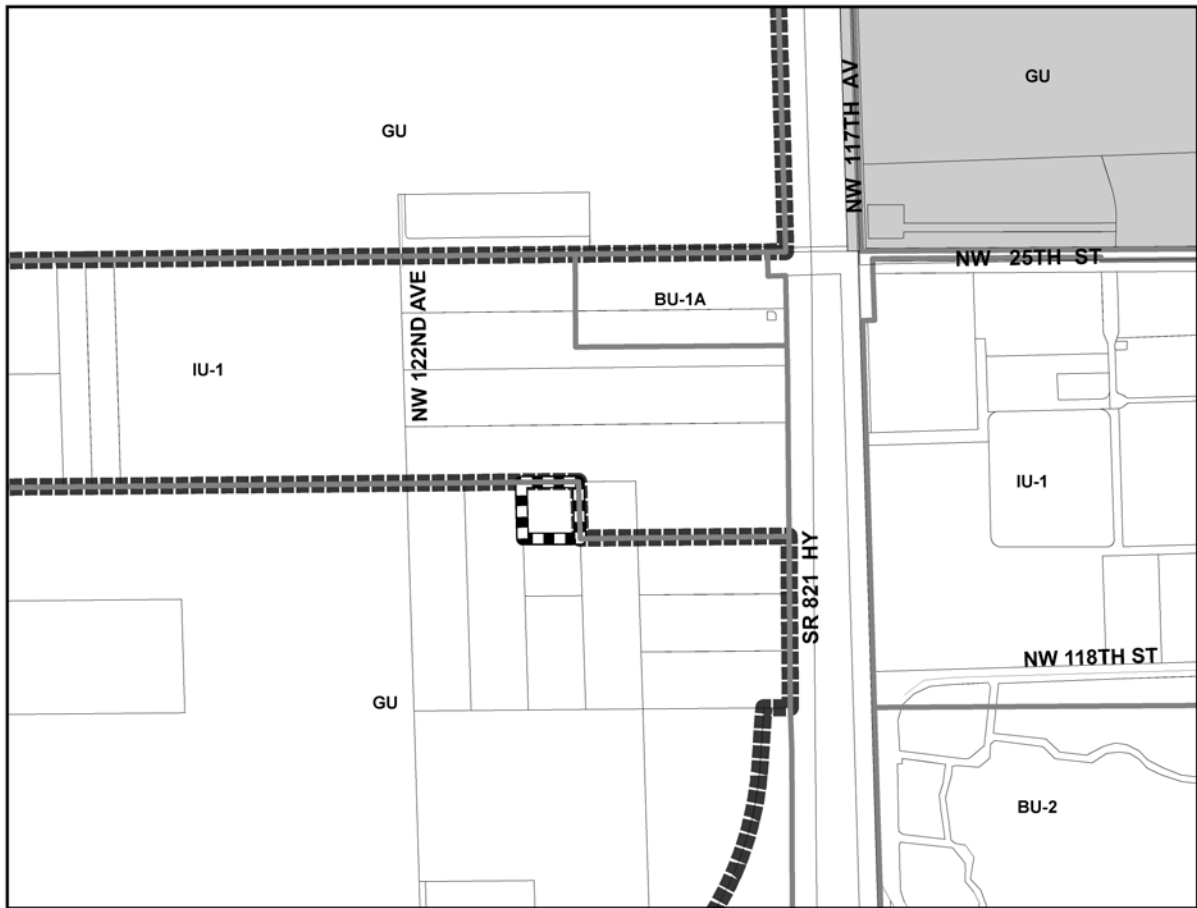
2005 URBAN DEVELOPMENT BOUNDARY








SOURCE: MIAMI-DADE COUNTY, DEPARTMENT
OF PLANNING AND ZONING, 2005



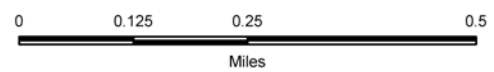
Figure C-3
APPLICATION NO. 6
CURRENT ZONING MAP



-  APPLICATION AREA
-  2005 URBAN DEVELOPMENT BOUNDARY
-  2015 URBAN EXPANSION AREA
-  UNINCORPORATED MIAMI-DADE
-  CITY OF DORAL

MIAMI-DADE ZONING DISTRICTS

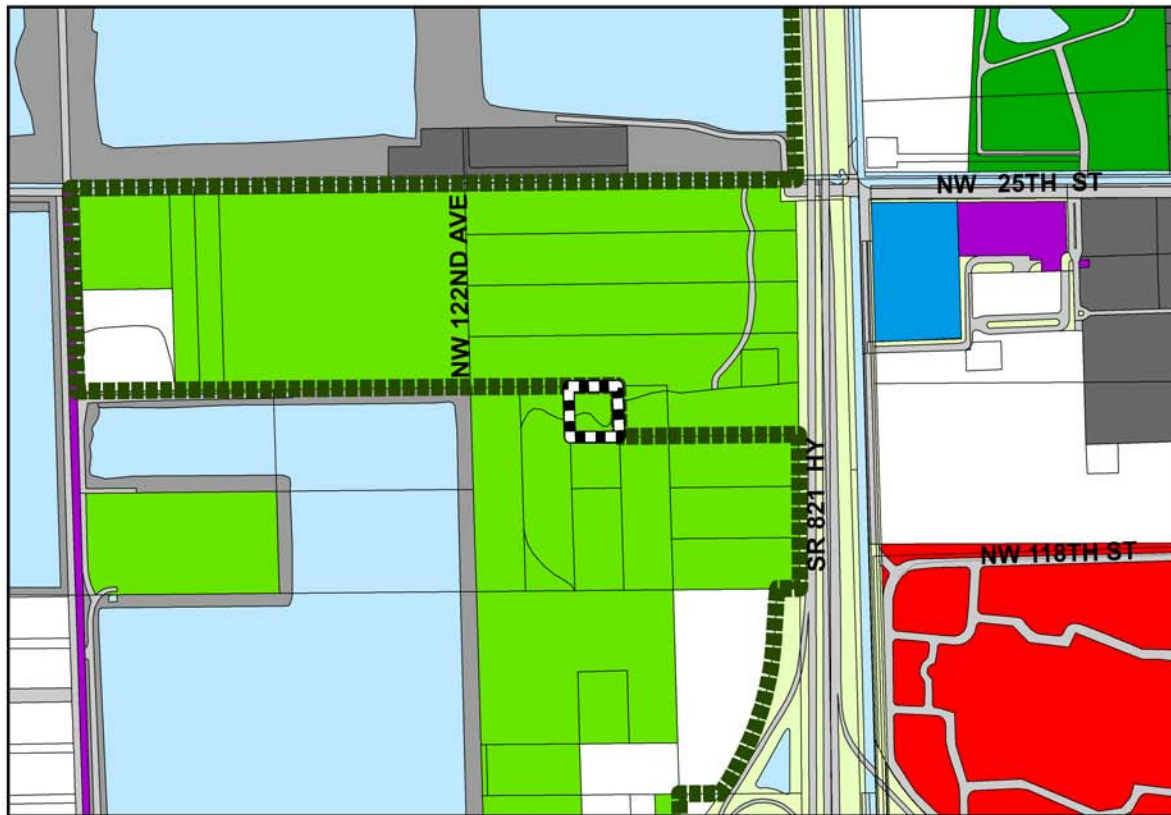
- GU INTERIM - USES DEPEND ON CHARACTER OF NEIGHBORHOOD, OTHERWISE EU-2 STANDARD APPLY
- BU-1A LIMITED BUSINESS
- BU-2 SPECIAL BUSINESS
- IU-1 INDUSTRIAL-LIGHT



SOURCE: MIAMI-DADE COUNTY, DEPARTMENT
OF PLANNING AND ZONING, 2005



Figure C-4
APPLICATION NO. 6
EXISTING LAND USE



APPLICATION AREA



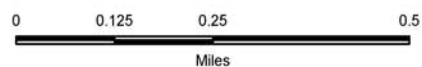
2005 URBAN DEVELOPMENT BOUNDARY

2003 EXISTING LAND USE

- SINGLE-FAMILY
- TWO-FAMILY DUPLEXES
- MOBILE HOME PARKS
- TOWNHOUSES
- LOW-DENSITY MULTI-FAMILY
- HIGH-DENSITY MULTI-FAMILY
- TRANSIENT-RESIDENTIAL (HOTEL, MOTEL)
- COMMERCIAL, SHOPPING CENTERS, STADIUMS
- OFFICE
- INSTITUTIONAL
- INDUSTRIAL EXTRACTION
- INDUSTRIAL
- AIRPORTS, PORTS

2003 EXISTING LAND USE (cont'd)

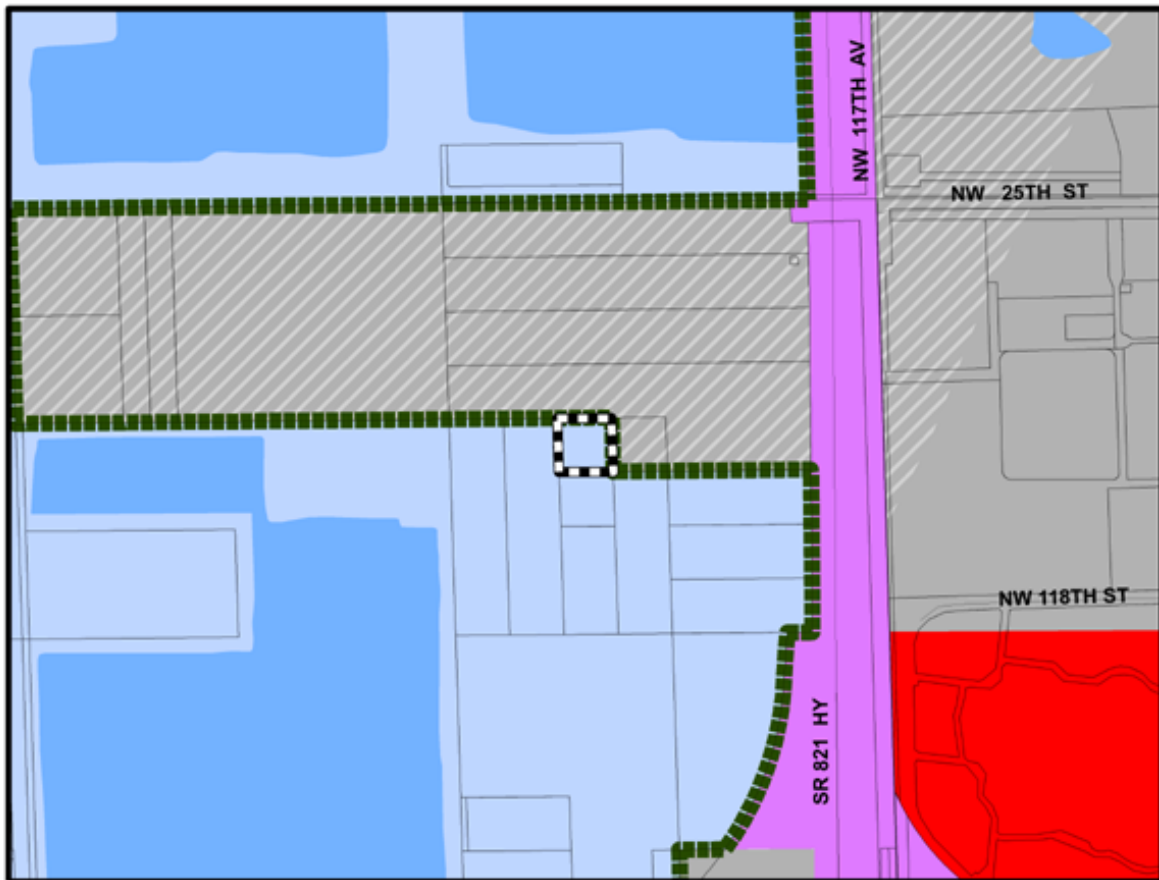
- COMMUNICATIONS, UTILITIES, TERMINALS
- STREETS, ROADS, EXPRESSWAYS, RAMPS
- STREETS, EXPRESSWAYS R/W
- AGRICULTURE
- PARKS, PRESERVES, CONSERVATION AREAS
- WATER CONSERVATION AREAS
- VACANT, GOVERNMENT OWNED
- VACANT, PROTECTED, PRIVATELY OWNED
- VACANT, UNPROTECTED
- INLAND WATERS
- OCEAN, BAY WATERS



SOURCE: MIAMI-DADE COUNTY, DEPARTMENT
OF PLANNING AND ZONING, 2005



Figure C-5
APPLICATION NO. 6
CDMP LAND USE PLAN



LEGEND



APPLICATION AREA



2005 URBAN DEVELOPMENT BOUNDARY

CDMP LAND USE



INDUSTRIAL AND OFFICE



RESTRICTED INDUSTRIAL AND OFFICE



BUSINESS AND OFFICE



INSTITUTIONAL AND PUBLIC FACILITY



OPEN LAND

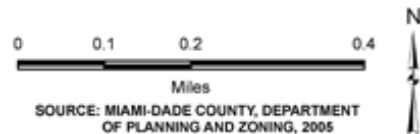


WATER



TRANSPORTATION

NOTE: This figure is a graphic representation drawn at a different scale than the Official Adopted 2005 and 2015 Land Use Plan (LUP) map, which was adopted at a scale of one inch to a mile. The LUP map with subsequent adopted amendments, governs where this figure differs.



SOURCE: MIAMI-DADE COUNTY, DEPARTMENT OF PLANNING AND ZONING, 2005

Application No. 7

This large-scale application site contains a total of approximately 21.6 acres. The application property lies outside the 2005 Urban Development Boundary (UDB), and is bounded on the south by the Tamiami Canal, on the east by Theoretical SW 138 Avenue (along the UDB boundary), on the north by theoretical SW 2 Street, and on the west by Theoretical SW 139 Avenue. The applicant is requesting that the land use of the application site be redesignated from “Open Land” to “Business and Office” and that the UDB be moved to the western boundary of the parcel.

Existing Land Use Pattern. Current zoning and the development pattern promoted by the CDMP Land Use Plan map are depicted in Figures C-6 through C-8. The application site currently consists of vacant property zoned GU-Interim use. Lands to the immediate north and west of the application site are vacant; however further to the north is the Silver Eagle Truck Storage facility. Much of the land to the west of the application site is in public ownership and may be used for stormwater management purposes. Land immediately to the east of the application site is vacant, but further east, beyond SW 137 Avenue, lies a large single-family home subdivision and electrical transmission lines. South of the application site, beyond the Tamiami canal and SW 8 Street, lies a Chevron gas station and the Tamiami Shops Shopping Center, a mixture of retail and other commercial uses.



Extensive areas located to the east and south of the application site are designated, zoned and developed as residential communities, most with a zoning of RU-1 (Single Family Residential), with the exception of the south frontage of Tamiami Trail (SW 8 Street) and a portion of the west frontage of SW 137 Avenue, which are predominantly zoned and developed for business uses. NW/SW 137 Avenue is currently being redeveloped into a 6-lane divided arterial north of the site in conjunction with the extension of the Dolphin Expressway, also under construction, to its NW 137 Avenue terminus. Lands surrounding these proposed transportation improvements are mainly zoned for industrial uses.

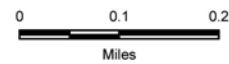
Future Development Pattern. The site and lands to the north and west (outside the UDB) are currently designated as “Open Land” on the LUP map. To the east and south of the site, the land use is largely designated as “Low Density Residential” (2.5 to 6.0 Dwelling Units per gross acre). A “Business and Office” strip is located along SW 8 Street, and immediately to the east of the application site along SW 137 Avenue. A “Low- Medium Density Residential” (5 to 13 Dwelling Units per gross acre) land use designation is located southeast of the application site, and an “Industrial and Office” land use designation is located to the northeast of the site. The portion of the CDMP Land Use Plan map that depicts the area surrounding this application site is included as Figure C-9.

Figure C-6
AERIAL PHOTO: APPLICATION NO. 7



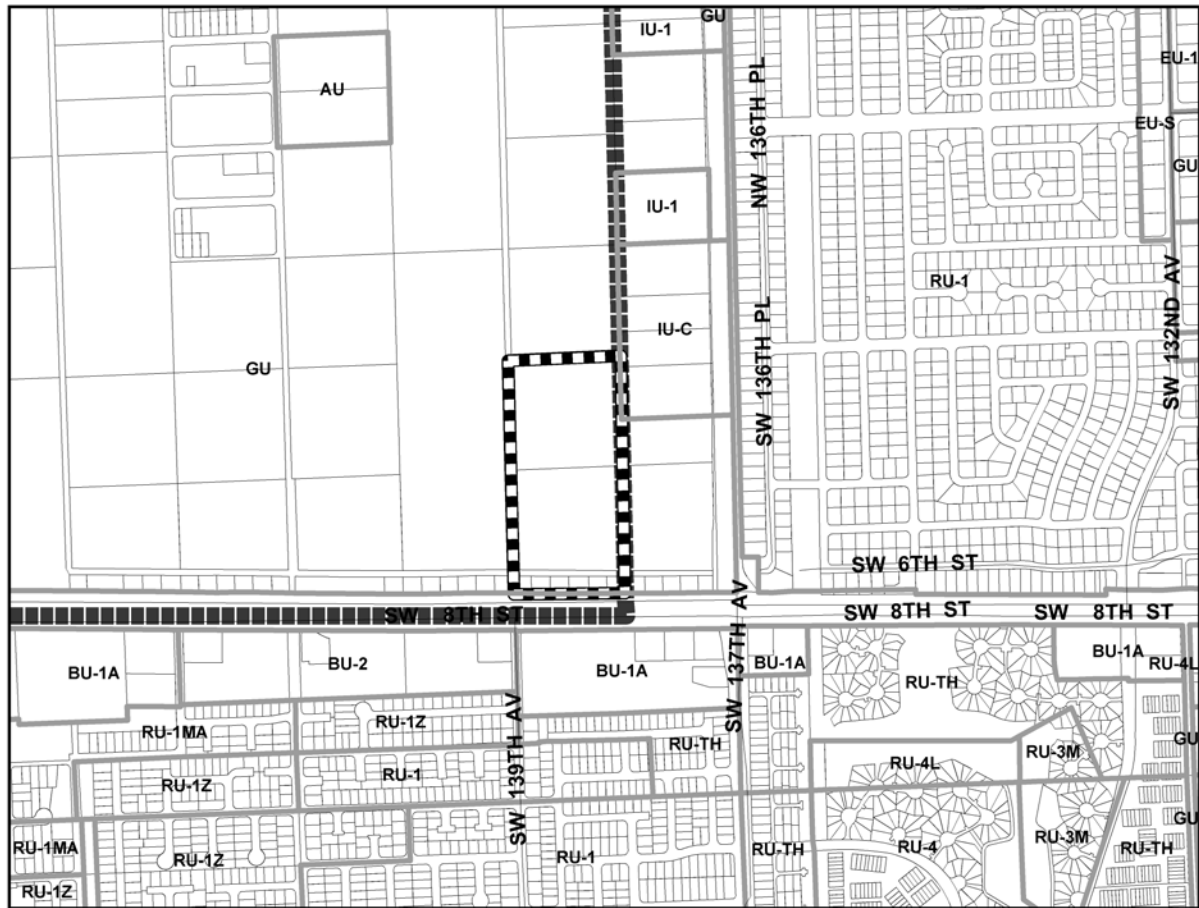
2005 AERIAL

-  APPLICATION AREA
-  2005 URBAN DEVELOPMENT BOUNDARY



SOURCE: MIAMI-DADE COUNTY, DEPARTMENT
OF PLANNING AND ZONING, 2005

Figure C-7
APPLICATION NO. 7
CURRENT ZONING MAP



APPLICATION AREA



2005 URBAN DEVELOPMENT BOUNDARY

MIAMI-DADE ZONING DISTRICTS

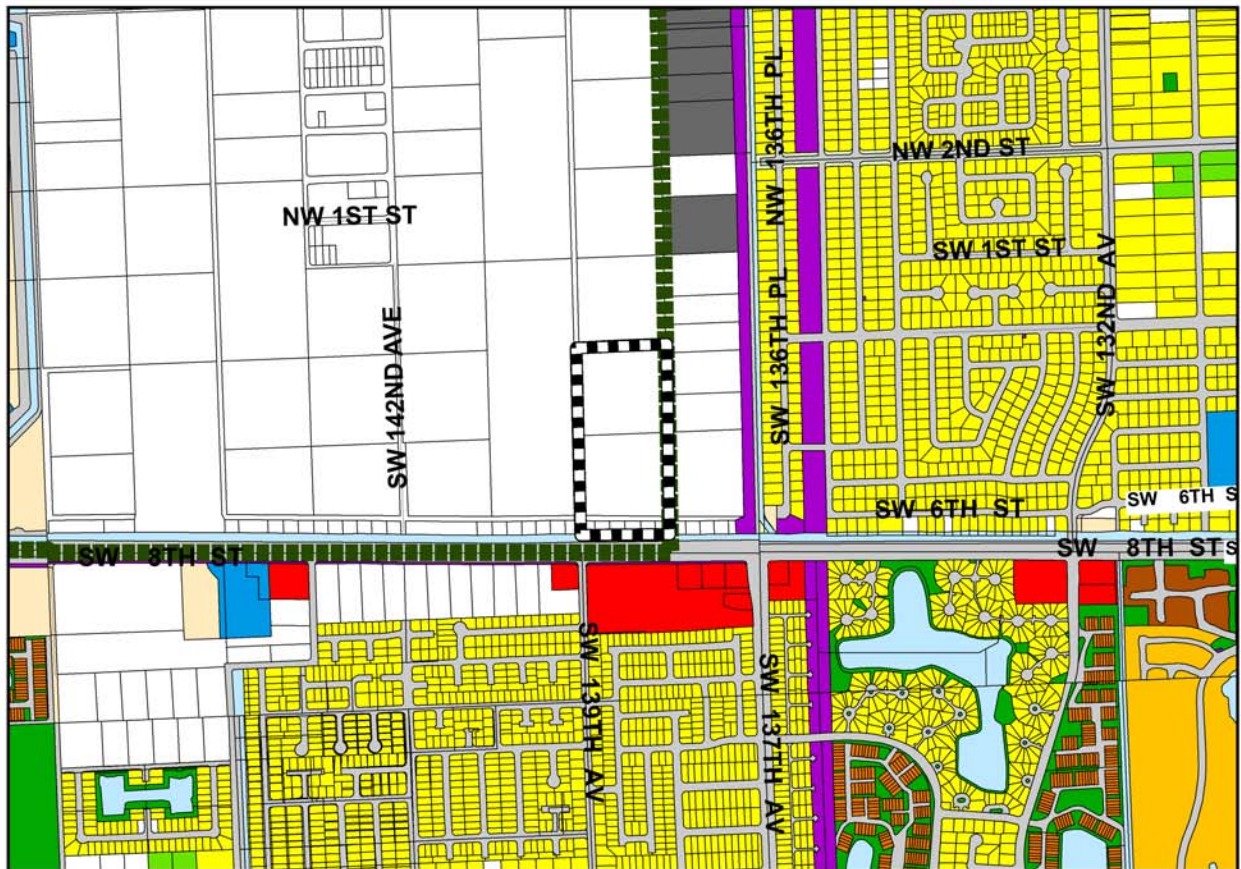
GU	INTERIM - USES DEPEND ON CHARACTER OF NEIGHBORHOOD, OTHERWISE EU-2 STANDARD APPLY
AU	AGRICULTURE - RESIDENTIAL 5 ACRES GROSS
RU-1	SINGLE FAMILY RESIDENCE 7,500 SQ. FT. NET
RU-1Z	SINGLE FAMILY ZERO LOT LINE 4,500 SQ. FT. NET
RU-1 (M) (a)	MODIFIED SINGLE FAMILY 5,000 SQ. FT. NET
RU-TH	TOWNHOUSE – 8.5 UNITS / NET ACRE
RU-3M	MINIMUM APARTMENT HOUSE 12.9 UNITS / NET ACRE
RU-4	APARTMENTS 50 UNITS / NET ACRE
RU-4L	LIMITED APARTMENTS HOUSE 23 UNITS / NET ACRE
BU-1A	LIMITED BUSINESS
IU-1	INDUSTRIAL-LIGHT
IU-C	INDUSTRIAL-CONDITIONAL

0 0.125 0.25 0.5
Miles

SOURCE: MIAMI-DADE COUNTY, DEPARTMENT
OF PLANNING AND ZONING, 2005



Figure C-8
APPLICATION NO. 7
EXISTING LAND USE MAP



APPLICATION AREA



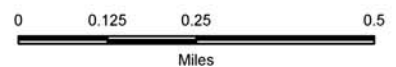
2005 URBAN DEVELOPMENT BOUNDARY

2003 EXISTING LAND USE

- SINGLE-FAMILY
- TWO-FAMILY DUPLEXES
- MOBILE HOME PARKS
- TOWNHOUSES
- LOW-DENSITY MULTI-FAMILY
- HIGH-DENSITY MULTI-FAMILY
- TRANSIENT-RESIDENTIAL (HOTEL, MOTEL)
- COMMERCIAL, SHOPPING CENTERS, STADIUMS
- OFFICE
- INSTITUTIONAL
- INDUSTRIAL EXTRACTION
- INDUSTRIAL
- AIRPORTS, PORTS

2003 EXISTING LAND USE (cont'd)

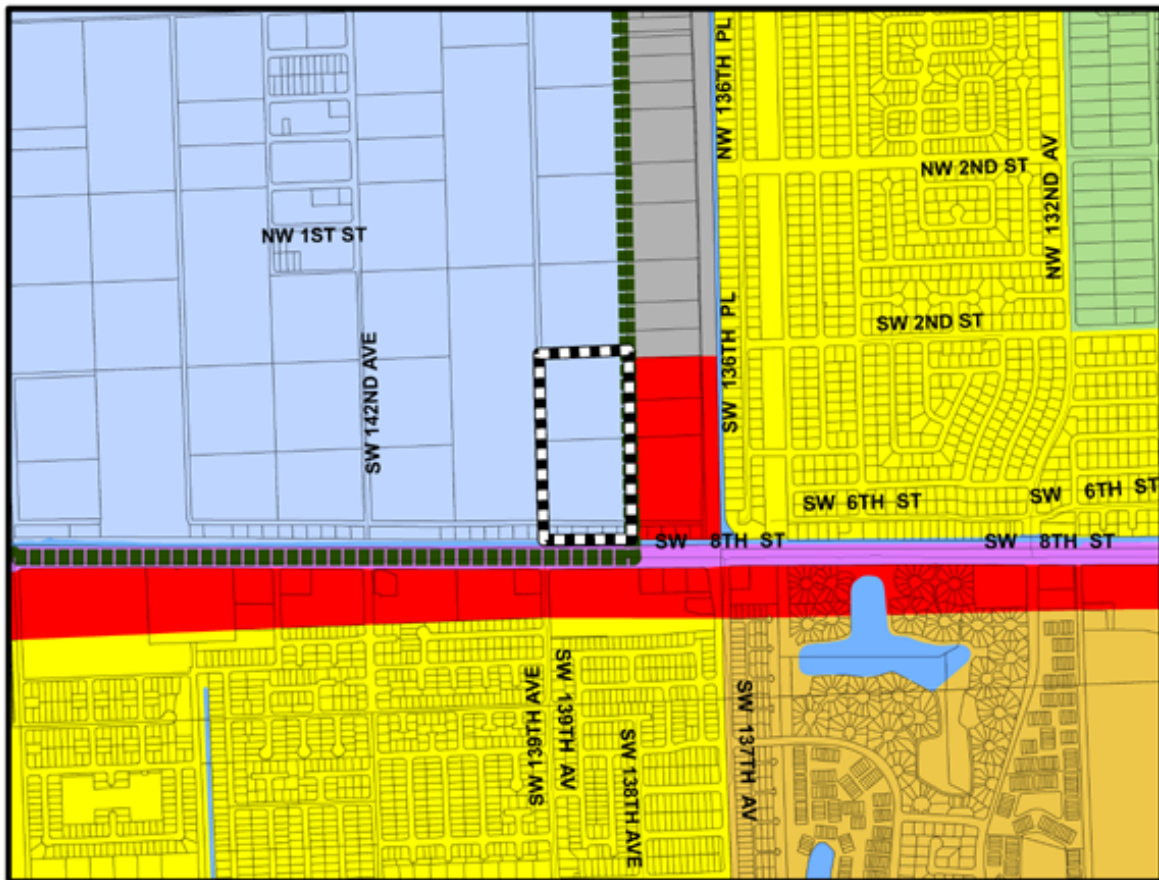
- COMMUNICATIONS, UTILITIES, TERMINALS
- STREETS, ROADS, EXPRESSWAYS, RAMPS
- STREETS, EXPRESSWAYS R/W
- AGRICULTURE
- PARKS, PRESERVES, CONSERVATION AREAS
- WATER CONSERVATION AREAS
- VACANT, GOVERNMENT OWNED
- VACANT, PROTECTED, PRIVATELY OWNED
- VACANT, UNPROTECTED
- INLAND WATERS
- OCEAN, BAY WATERS



SOURCE: MIAMI-DADE COUNTY, DEPARTMENT OF PLANNING AND ZONING, 2005



Figure C-9
APPLICATION NO. 7
CDMP LAND USE PLAN



LEGEND



APPLICATION AREA



2005 URBAN DEVELOPMENT BOUNDARY

CDMP LAND USE

RESIDENTIAL COMMUNITIES



ESTATE DENSITY RESIDENTIAL (EDR) 1-2.5 DU/AC



LOW DENSITY RESIDENTIAL (LDR) 2.5-6 DU/AC



LOW-MEDIUM DENSITY RESIDENTIAL (LMDR) 5-13 DU/AC



INDUSTRIAL AND OFFICE



BUSINESS AND OFFICE



OPEN LAND

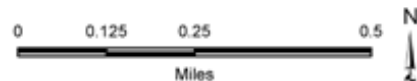


WATER



TRANSPORTATION

NOTE: This figure is a graphic representation drawn at a different scale than the Official Adopted 2005 and 2015 Land Use Plan (LUP) map, which was adopted at a scale of one inch to a mile. The LUP map with subsequent adopted amendments, governs where this figure differs.



SOURCE: MIAMI-DADE COUNTY, DEPARTMENT
OF PLANNING AND ZONING, 2005

Supply and Demand for Residential Land

Vacant residential land in Study Area C (Minor Statistical Area 3.2) in 2004 was estimated to have a capacity for about 15,200 dwelling units, with about 68 percent of this intended for multi-family type units. The large volume of multi-family land may be attributed to the recent land use plan changes initiated by the City of Doral in Section 8 and in connection with the Kroger Center. The annual average demand is projected to decrease from 1,031 units per year in the 2004-2010 period to only 107 units per year in the 2015-2020 period. An analysis of the residential capacity without differentiating by type of unit shows absorption occurring beyond the year 2025 (See Table C-3). More than three quarters of the projected demand is for single-family type units, and this land is projected to be absorbed by the year 2010. The supply of multifamily land is projected to accommodate demand beyond 2025 because demand is projected to be low and declining.

Table C-3
Residential Land Supply/Demand Analysis
2003 to 2025: Study Area C

ANALYSIS DONE SEPARATELY FOR EACH TYPE, I.E. NO SHIFTING OF DEMAND BETWEEN SINGLE & MULTI-FAMILY TYPE	STRUCTURE TYPE		
	SINGLE-FAMILY	MULTI-FAMILY	BOTH TYPES
CAPACITY IN 2003	4,856	10,330	15,186
DEMAND 2003-2010	943	294	1,237
CAPACITY IN 2010	141	8,860	9,001
DEMAND 2010-2015	738	231	969
CAPACITY 2015	0	7,705	4,155
DEMAND 2015-2020	82	25	107
CAPACITY 2020	0	7,580	3,620
DEMAND 2020-2025	0	0	0
CAPACITY 2025	0	7,580	3,620
DEPLETION YEAR	2010	>2025	>2025

Residential capacity is expressed in terms of housing units. Housing demand is an annual average figure based on proposed population projections.

Depletion year = 9999 when capacity remains but there is no demand projected.

Source: Miami-Dade Department of Planning and Zoning, Planning Research Section, 2005.

The table above addresses residential land supply and demand in Study Area C without the effect of the proposed CDMP amendments. There are two proposed amendments in this area (Application Nos. 6 and 7). Application No. 6 requests the redesignation of 2.5 acres from Open Land to Restricted Industrial and Office and Application No. 7 requests the redesignation of 21.6 acres from Open Land to Business and Office use. The approval of either application would have no impact on the residential capacity of the area.

Supply and Demand for Commercial Land

Study Area C (MSA 3.2) contained 429.3 acres of vacant land zoned or designated for commercial uses in the year 2004. The average annual absorption rate projected for the 2003-2025 period is 17.2 acres per year. At the projected rate of absorption, the study area will deplete its supply of commercial zoned or designated land beyond the year 2025. In addition, its commercial acres per thousand persons ratio exceeds the County average for both 2015 and 2025 (See Table C-4).

Table C-4
Projected Absorption of Land for Commercial Uses
Indicated Year of Depletion and Related Data
Study Area C

Study Area C MSA 3.2	Vacant Commercial Land 2004 (Acres)	Commercial Acres in Use 2004	Annual Absorption Rate 2003-2025 (Acres)	Projected Year of Depletion	Total Commercial Acres per Thousand Persons	
					2015	2025
Total	429.3	1,526.3	17.2	2025+	11.6	11.5

Source: Miami-Dade Department of Planning & Zoning, Planning Research Section, July 2005.

Supply and Demand for Industrial Land

At the beginning of 2004, the existing supply of vacant industrial land in Study Area C (MSA 3.2) consisted of 1,999.4 acres. The absorption of such land over the 2003 to 2025 period is projected at an average annual rate of 68.71 acres. This and other related data are shown in Table C-5 below. Based on the projected rate of absorption reflecting the past rate of such uses, the existing supply of industrial zoned land in the study area would last until the year 2022.

Table C-5
Projected Absorption of Land for Industrial Uses
Indicated Year of Depletion and Related Data
Study Area C

MSA	Vacant Industrial Land 2004 (Acres)	Industrial Land in Use 2004 (Acres)	Average Annual Absorption Rate 2003 thru 2025 (Acres)	Indicated Year of Depletion
3.2	1,999.4	5,179.30	68.71	2022
Study Area	1,999.4	5,179.30	68.71	2022

Source: Miami-Dade County, Department of Planning and Zoning, Planning Division, Research Section, August 2005.

Roadways

Existing Conditions

Figure C-10 depicts the existing arterial roadway network serving this Truncated Study Area. East-west expressways and arterials include the Dolphin Expressway (SR 836), SW 8 Street (SR 90), Flagler Street, NW 12, NW 25, NW 36/41, and NW 58 Streets. North-south expressways and arterials include Krome Avenue (SR 997), the Homestead Extension of the Florida Turnpike (HEFT)/(SR 821), NW 137, NW 127, NW 107 (SR 985), and NW 97 Avenues.

Table C-6 lists and Figure C-11 shows the existing operating peak-period levels of service (LOS) for roadways in this Truncated Study Area. Current traffic conditions on major roadways in this area are relatively uncongested, LOS C or better. However, NW/SW 122 Avenue, from NW 6 Street to SW 8 Street, is operating at LOS F, and NW/SW 177 Avenue (Krome Ave.), from Okeechobee Road to SW 8 Street, is operating at LOS D, surpassing the adopted LOS C and LOS E standards, respectively, for these roadways. All other expressways and arterials that are currently monitored show acceptable peak period LOS conditions.

Traffic Concurrency Evaluation

The Truncated Study Area is located partly within the County's 2005 Urban Development Boundary (UDB), and partly within the 2015 Urban Expansion Area, with the western area situated largely outside of the UDB. A recent evaluation of peak-period traffic concurrency conditions in this Truncated Study Area as of July 12, 2005, which considers reserved trips from approved development not yet constructed and programmed roadway capacity improvements, predicts that the following roadway segments will run out of concurrency service capacity:

Roadway Segment	Trip Deficiency
Krome Avenue between Okeechobee Road to SW 8 Street	-153
NW 12 Street between NW 112 Avenue and NW 107 Avenue	-458
NW 12 Street between NW 117 Avenue and NW 127 Avenue	-13
NW 25 Street between NW 107 Avenue to NW 97 Avenue	-41
NW 58 Street between NW 107 Avenue and NW 117 Avenue	-5
NW 107 Avenue between NW 41 Street and NW 58 Street	-75
NW/SW 127 Avenue between NW 6 Street and SW 8 Street	-264
SR 836 (Dolphin Expressway) between NW 107 Avenue and NW 87 Avenue	-186

Source: Miami-Dade County Public Works Department, July 2005; Florida Department of Transportation, July 2004.

Figure C-12 shows the concurrency levels of services for roadways in this Truncated Study Area and those roadway segments that will exceed the adopted LOS standards applicable to this area

Figure C-10
ROADWAYS: Application Nos. 6 & 7

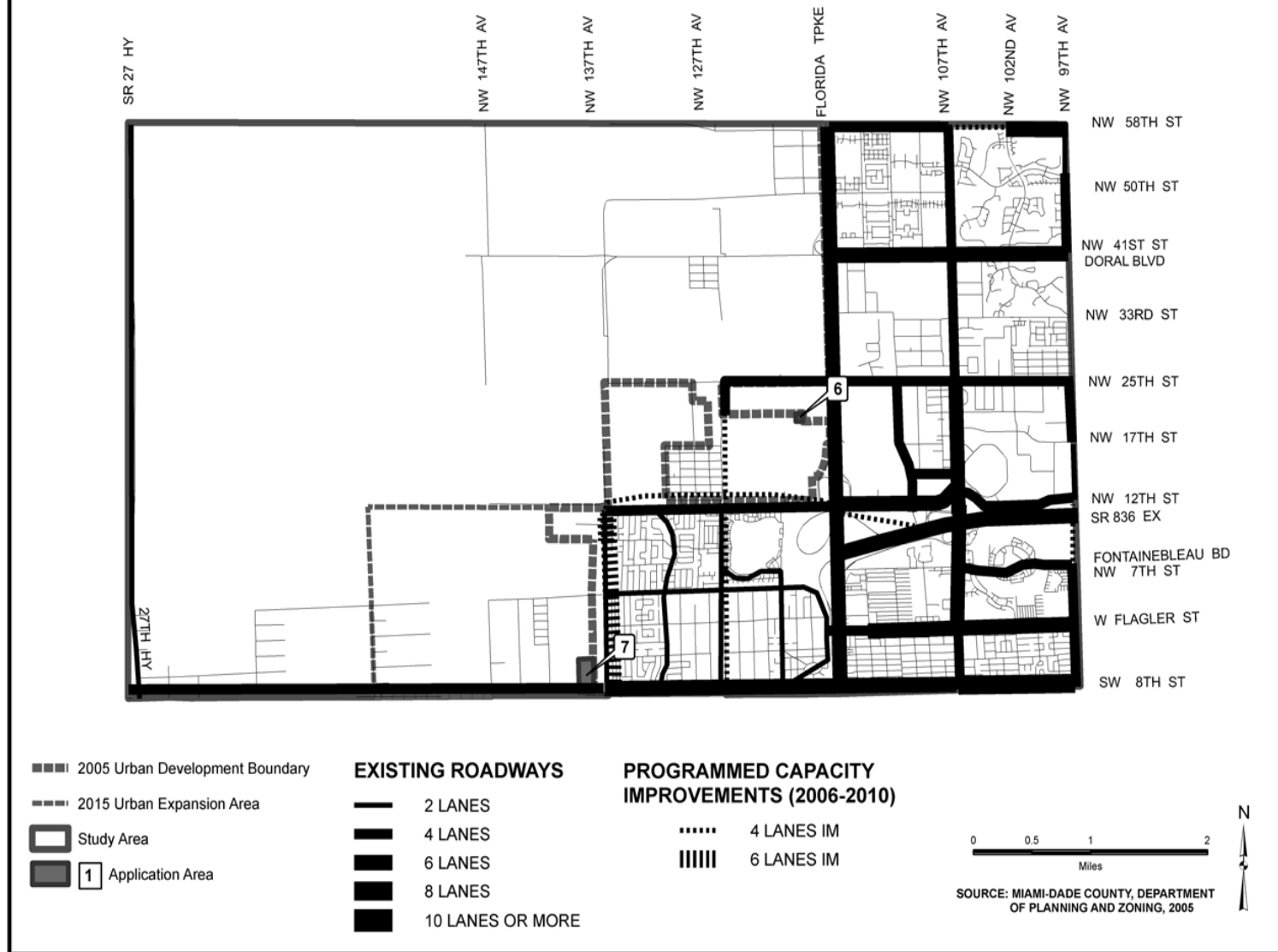


Figure C-11
EXISTING ROADWAYS LEVEL OF SERVICE: Application Nos. 6 & 7

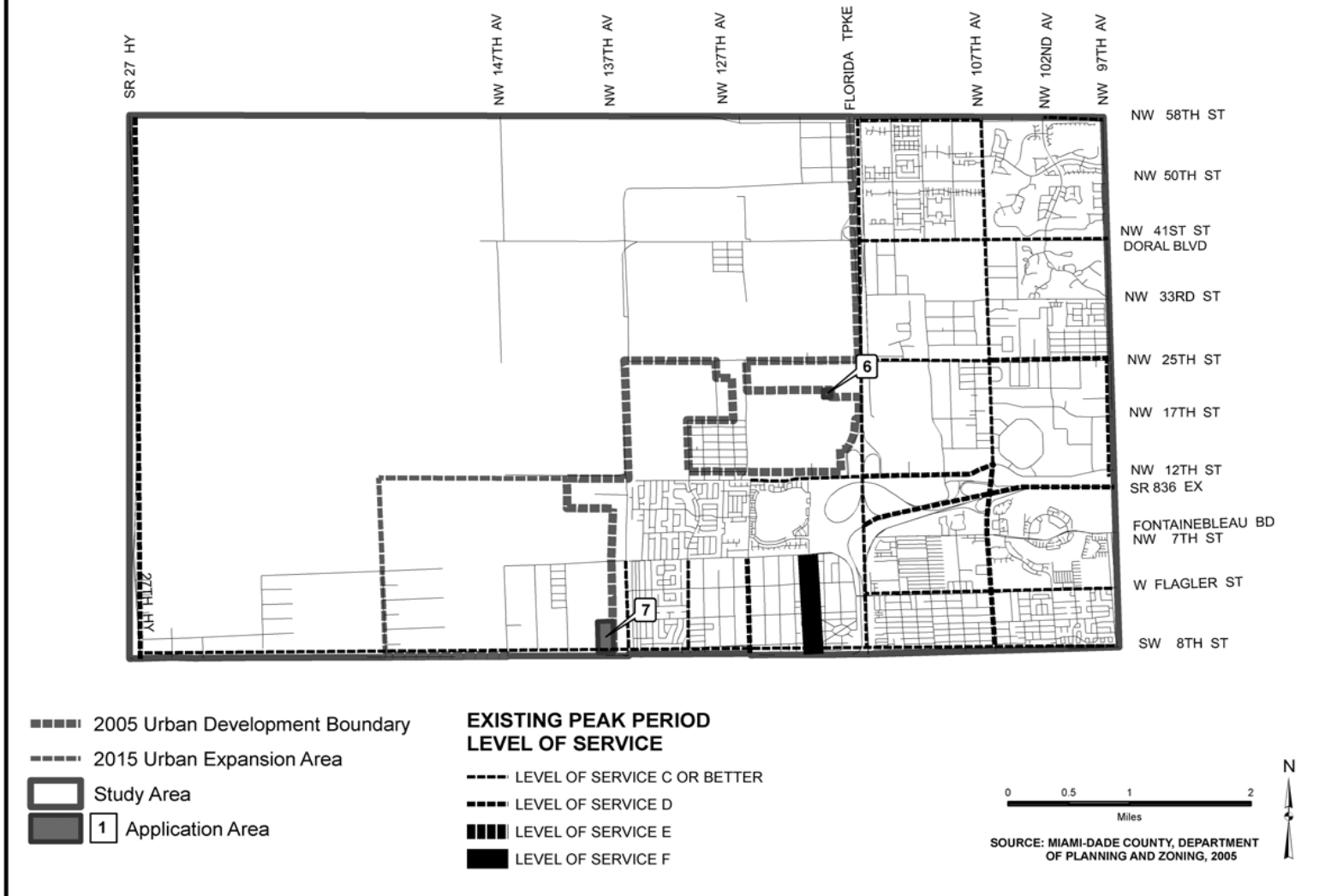


Table C-6
Existing Traffic Conditions
Roadway Lanes and Peak Period Level of Service (LOS)

Roadway	Location/Link	Lanes	LOS Std.*	LOS
NW/SW 177 Ave./Krome Ave.	SW 8 Street to Okeechobee Road	2 UD	C	D (01)
NW/SW 137 Avenue	NW 6 Street to SW 8 Street	2 UD	D	C (04)
NW/SW 132 Avenue	NW 6 Street to SW 8 Street	2 UD	D	A (04)
NW/SW 127 Avenue	NW 6 Street to SW 8 Street	2 UD	D	D (04)
NW/SW 122 Avenue	NW 6 Street to SW 8 Street	2 UD	E	F (04)
SR 821 (HEFT)	Okeechobee Road to SR 836	6 LA	D	A (98)
	SR 836 to SW 8 Street	6 LA	D	C (96)
NW/SW 107 Avenue	NW 58 Street to NW 41 Street	4 DV	D	B (04)
	NW 41 Street to NW 25 Street	4 DV	D	A (04)
	NW 25 Street to NW 12 Street	6 DV	D	B (04)
	NW 12 Street to SR 836	6 DV	D	B (04)
	West Flagler Street to SR 836	6 DV	E	D (00)
	SW 8 Street to W. Flagler St.	4DV	E	D (01)
NW 97 Avenue	NW 25 Street to NW 12 Street	4 DV	D	B (04)
NW 58 Street	NW 117 Avenue to NW 107 Avenue	4 DV	D	A (04)
	NW 102 Avenue to NW 97 Avenue	4 DV	D	A (04)
NW 36 / 41 Street (Doral Blvd.)	SR 821 (HEFT) to NW 107 Avenue	6 DV	D	A (04)
	NW 107 Avenue to NW 97 Avenue	6 DV	D	B (04)
NW 25 Street	NW 117 Avenue to NW 107 Avenue	4DV	D	B (04)
	NW 107 Avenue to NW 97 Avenue	4 DV	D	D (04)
NW 12 Street	NW 127 Ave. to NW 117 Ave.	4 DV	D	B (04)
	NW 117 Avenue to NW 107 Avenue	6 DV	D	D (01)
SR 836/ (Dolphin Expressway)	SR 821 (HEFT) to NW 107 Avenue	6 LA	D	D (01)
	NW 107 Avenue to NW 87 Avenue	6 LA	D	D (01)
West Flagler Street	W 118 Avenue to W 114 Avenue	4 DV	E+20%	A (04)
	W 114 Avenue to W 107 Avenue	6 DV	E+20%	B (04)
	W 107 Avenue to W 97 Avenue	6 DV	E+20%	C (04)
SW 8 Street	SW 177 Avenue to SW 137 Avenue	4 DV	C	C (00)
	SW 137 Avenue to SW 127 Avenue	6 DV	D	A (01)
	SW 127 Avenue to SR 821 (HEFT)	6 DV	E	C (02)
	SR 821 (HEFT) to SW 107 Avenue	6 DV	D	C (00)
	SW 107 Avenue to SW 87 Avenue	8 DV	E+20%	C (01)

Source: Miami-Dade County Public Works Department, Florida Department of Transportation, July 2005.

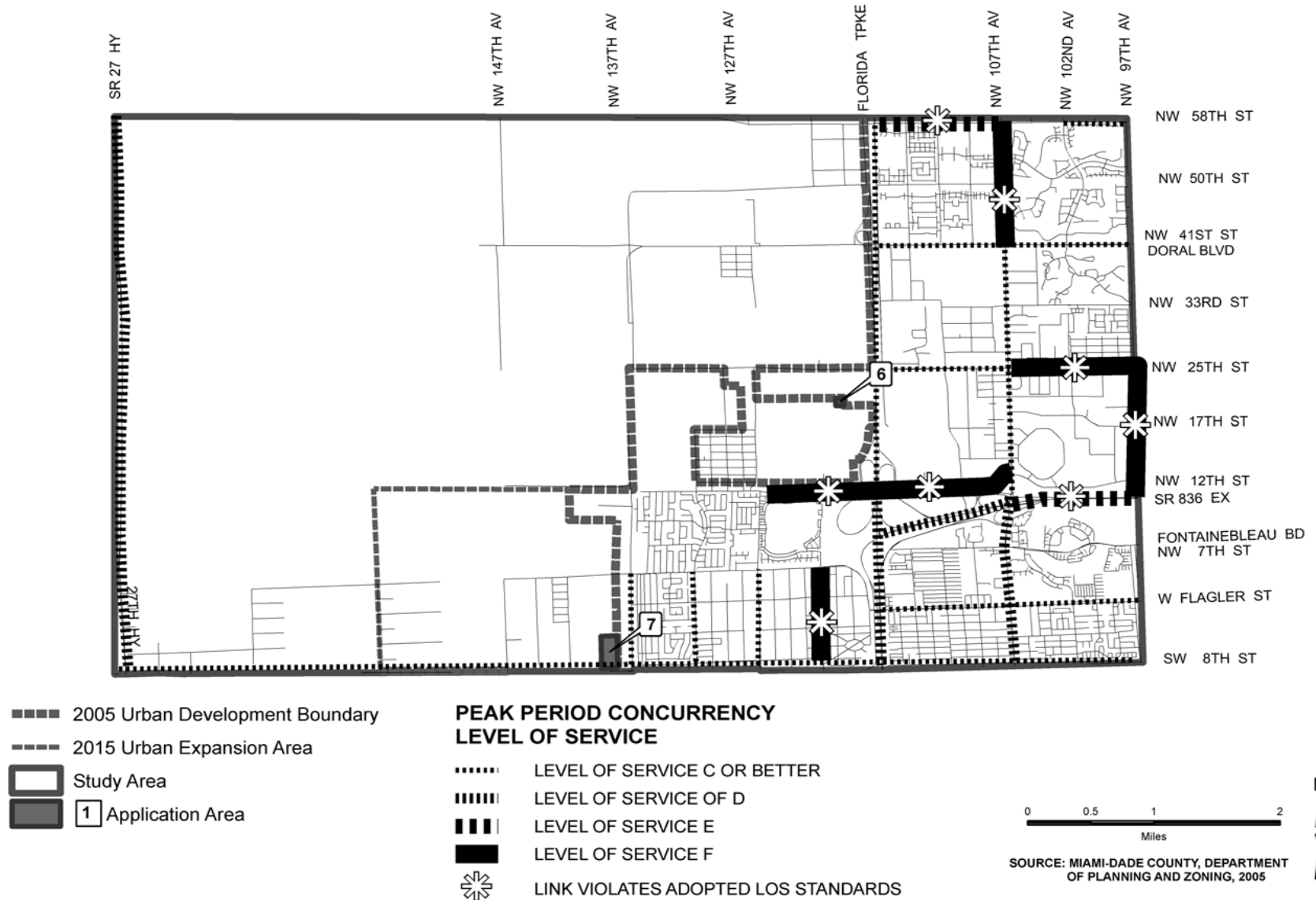
Notes: () in LOS column identifies year traffic count was updated or LOS traffic analysis revised

DV = Divided Roadway, UD = Undivided Roadway, LA=Limited Access

* Adopted minimum acceptable peak-period Level of Service Standard for roadway

E+20% means 120% of roadway capacity (LOS E), 20 minutes transit headway.

Figure C-12
ROADWAYS CONCURRENCY LEVEL OF SERVICE: Application Nos. 6 & 7



Future Conditions

Table C-7 lists and Figure C-10 shows the capacity improvements programmed for construction within this Truncated Study Area for Fiscal Years 2006-2010. Various significant projects are already under construction, including the six-lane widening of NW 137 Avenue, from NW 12 Street to SW 8 Street, the four-lane extension of SR 836 between the HEFT and NW 137 Avenue, and the new four-lane extensions of NW 127 Avenue, NW 25 Street and NW 58 Street.

Table C-7
Programmed Roadway Capacity Improvements
Fiscal Year 2006-2010
Truncated Study Area C

Roadway	From	To	Type of Improvement	Year
NW 97 Avenue		Bridge over SR 836	New 4-Lane Bridge	UC
NW 127 Avenue	NW 25 Street	NW 12 Street	New 4-Lane Roadway	UC
NW 137 Avenue	NW 12 Street	SW 8 Street	New 6-Lane Roadway	UC
NW 25 Street	NW 117 Avenue	NW 127 Avenue	New 4-Lane Roadway	UC
NW 58 Street	NW 107 Avenue	NW 102 Avenue	Widen to 4 lanes	UC
SR 836 (Dolphin Expressway)	NW 137 Avenue	SR 821 (HEFT)	New 6-Lane Roadway	UC
SR 836 (Dolphin Expressway)	SR 821 (HEFT)	SR 836 / SR 826 Interchange	New 4-Lane Divided Express Lanes	2009-10

Source: Transportation Improvement Program 2006, Miami-Dade County Metropolitan Planning Organization, June 2005.

Note: UC means under construction

A number of additional roadway improvements are planned for this Truncated Study Area by the year 2015, as indicated in Table C-8 below. These are Priority I and II projects with construction planned between 2005 and 2015. The year 2015-planned roadway network for this Truncated Study Area is shown in Figure C-13, assuming implementation of all programmed and planned roadway capacity improvements.

Figure C-13
PLANNED YEAR 2015 ROADWAY LANES: APPLICATION NOS. 6 & 7

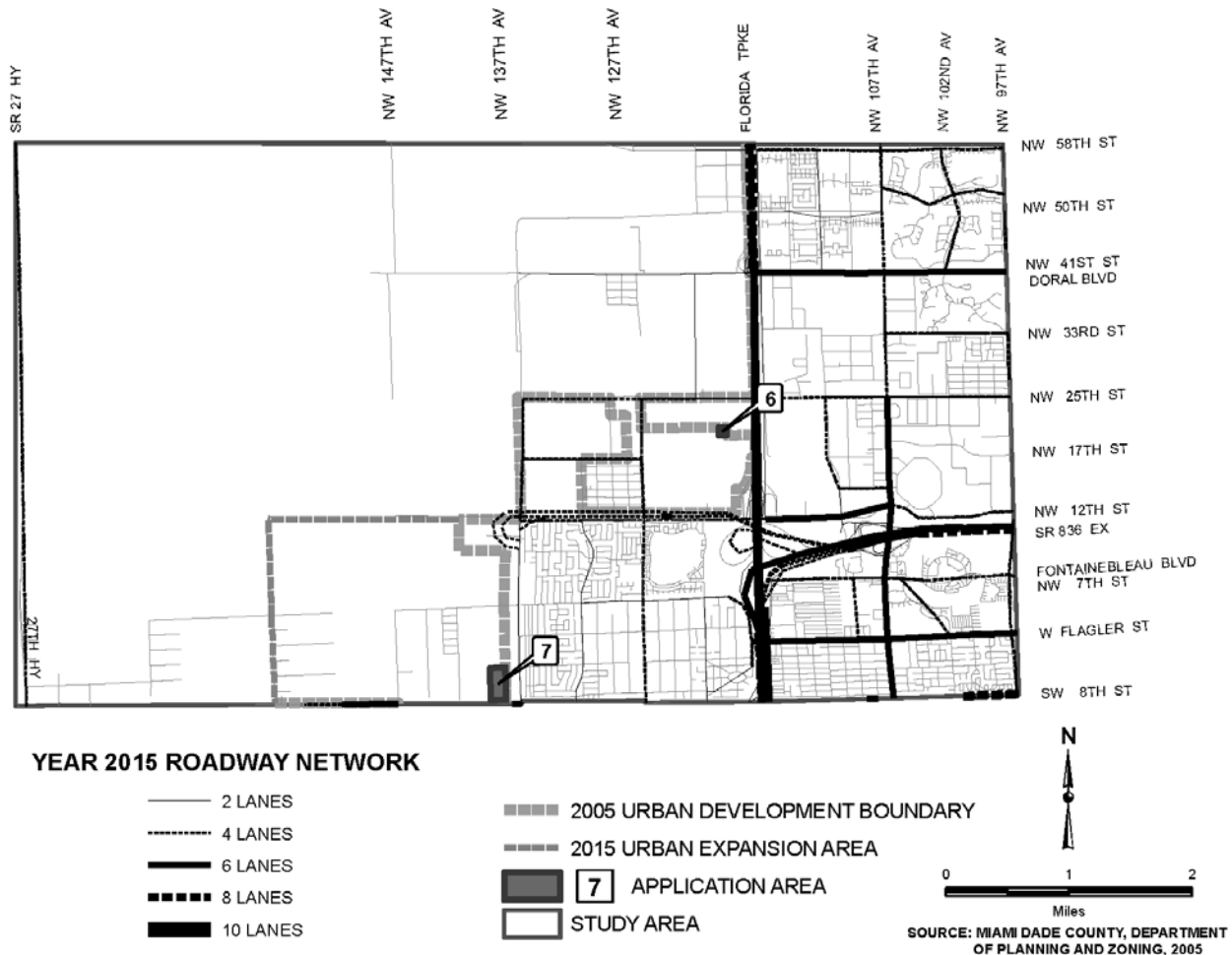


Table C-8
Planned Roadway Capacity Improvements
Fiscal Years 2006-2015
Truncated Study Area C

Roadway	From	To	Type of Improvement	Fiscal Year
SR 836 (Dolphin Expressway)	SR 821 (HEFT)	NW 107 Avenue	Widen SR 836/SR 821 Connection by 1 Lane	I
SR 826 (Palmetto Expressway)	SW 16 Street	SW 2 Street	Widen to 10 Lanes	I
NW 177 Ave/ Krome Ave	US 27/Okeechobee Rd	SW 8 Street	Access Mgmt/Safety	I
NW 137 Avenue	NW 12 Street	NW 17 Street	New 4-Lane Roadway	I*
NW 132 Place	NW 17 Street	NW 25 Street	New 4-Lane Roadway	I*
NW/SW 127 Avenue	SW 8 Street	NW 12 Street	Widen to 4 Lanes	I*
NW 122 Avenue	NW 41 Street	NW 25 Street	New 2-Lane Roadway	I*
NW 97 Avenue	NW 41 Street	NW 25 Street	Widen to 4 Lanes	I
NW 58 Street	NW 107 Avenue	NW 102 Avenue	Widen to 4 Lanes	I
NW 25 Street	NW 132 Place	NW 127 Avenue	New 2-Lane Roadway	I*
	NW 137 Avenue	NW 132 Place	New 2-Lane Roadway	I*
NW 17 Street	NW 127 Avenue	NW 137 Avenue	New 4-Lane Roadway	I*
SW 107 Avenue	SW 8 Street	W. Flagler Street	Widen to 6 Lanes	II

Source: Miami-Dade Transportation Plan to the Year 2030, Miami-Dade County Metropolitan Planning Organization, December 2004.

Notes: Priority I – Project improvements scheduled to be funded by the year 2009

Priority II – Project improvements planned to be funded between 2010 and 2015

* Roadway improvement by private sector. Construction of improvements are normally not linked to specific dates, but instead are usually dependent upon the construction schedule.

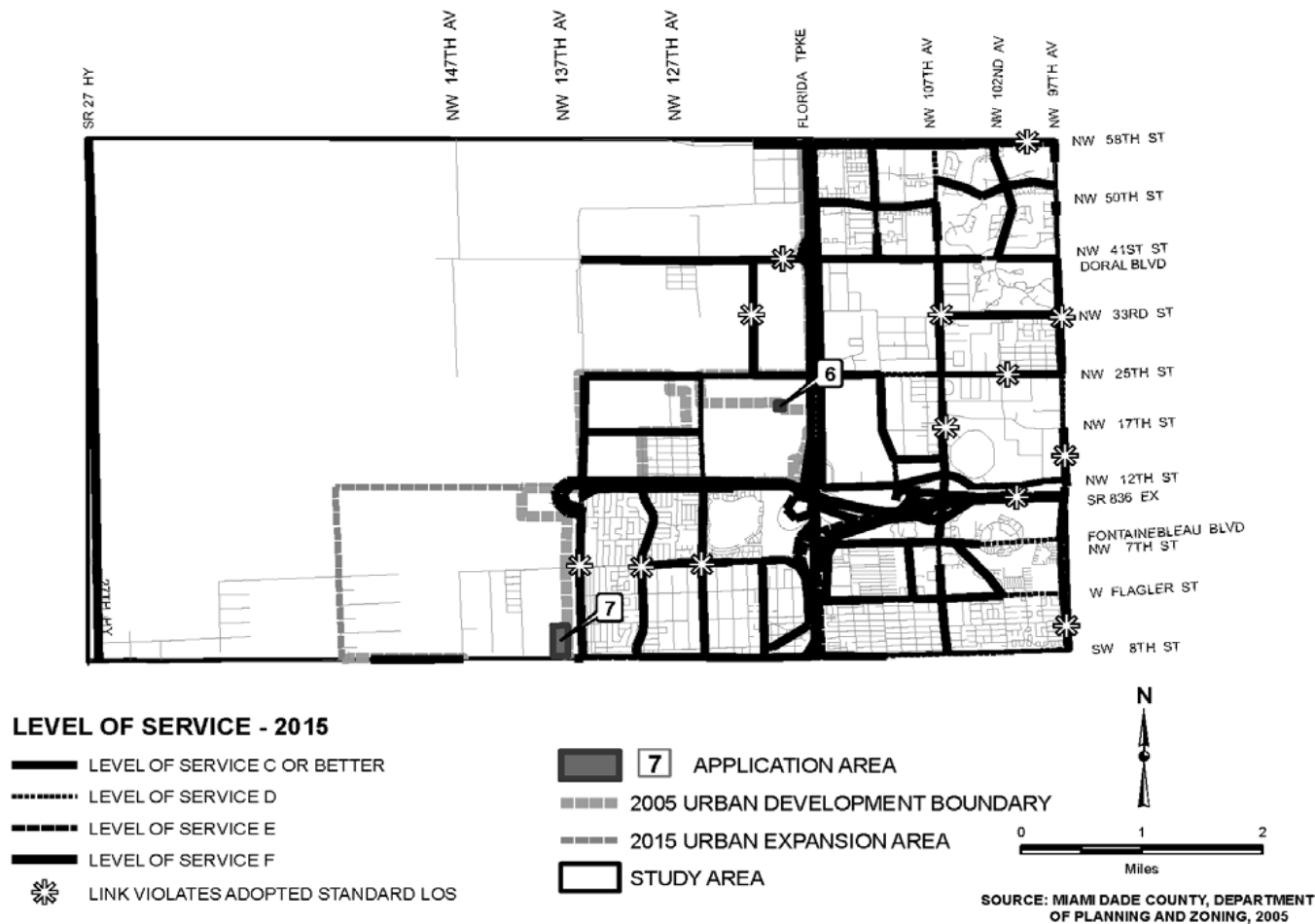
Figure C-14 shows the projected year 2015 levels of service for all roadways in the Truncated Study Area. Roadway segments in the vicinity of Application Nos. 6 and 7 that are projected to deteriorate to LOS F by the year 2015 are identified in Table C-9, shown below.

Table C-9
2015 Volume-to-Capacity (V/C) Ratios
Roadways Projected to Deteriorate To Level of Service F

Roadway Segment	V/C Ratio Without Applications	V/C Ratio With Applic. No. 7
SW/NW 137 Ave. from SW 8 Street to NW12 Street	1.00 – 1.62	1.09 – 1.67
SW/NW 132 Ave. from SW 8 Street to NW 12 Street	1.41 – 1.57	1.58 – 1.72
SW/NW 127 Ave. from SW 8 Street to NW 12 Street	1.14 - 1.50	1.20 – 1.58
NW 107 Ave. from NW 12 Street to SR 836	.94 - 1.43	1.06 – 1.63
SW 8 Street from SW 127 Ave. to SR 821 (HEFT)	1.13 – 1.33	1.15 – 1.35
SR 836 from SR 821 (HEFT) to SW 97 Avenue	1.00 – 1.15	1.00 – 1.17

Source: Gannett Fleming, Inc., Metropolitan Planning Organization, July 2005.

Figure C-14
PROJECTED YEAR 2015 LEVEL OF SERVICE: APPLICATION NOS. 6 & 7



Application Impacts

Application No. 6 is a 2.5-acre site located west of the HEFT and east of Theoretical NW 122 Avenue at approximately Theoretical NW 22 Street. There is currently no access to this “landlocked” site, and any points of access would have to be negotiated with adjacent property owners. Roadway sections in the immediate vicinity of the application site are currently operating at acceptable levels of service, LOS C or better, with the exception of NW/SW 122 Avenue between NW 6 and SW 8 Streets, which is operating at LOS F, violating the adopted LOS E standard for this roadway. Traffic concurrency analysis indicates that NW 25 Street between NW 107 and NW 97 Avenues, east of the Application, is predicted to operate at LOS F, thus violating the adopted LOS D standard applicable to this roadway. In analyzing the potential trip distribution of the trips generated by the proposed application, the proposed use will further deteriorate the level of service of NW 25 Street, from NW 107 and NW 97 Avenues, from LOS F (1.01) to LOS F (1.02). However, in the year 2015, no roadway segments in the immediate vicinity of the application site are projected to operate at LOS F.

Table C-10 identifies the estimated number of PM peak hour trips to be generated by the proposed warehouse development under the requested Land Use designation of Restricted Industrial and Office. It shows that if the site were developed as proposed, it would generate approximately 40 more PM peak-hour trips than under the current CDMP designation.

Application No. 7 is a 21.6-acre site located north of SW 8 Street, south of SW 2 Street, east of SW 139 Avenue, and west of Theoretical SW 138 Avenue. Access to this site, if approved, would be from those roads. Roadway sections in the immediate vicinity of the application site are currently operating at acceptable levels of service, LOS C or better. Traffic concurrency analysis indicates that the addition of trips generated by the proposed application will not significantly impact the levels of service of the adjacent roadways. Moreover, in the year 2015, no roadway segments in the immediate vicinity of the application site are projected to operate at LOS F.

Table C-10 below identifies the estimated number of PM peak hour trips expected to be generated by the proposed development under the requested land use designation (Business and Office) and compares it to the development that could occur (single family dwellings) under the current CDMP designation (Open Land). Application No. 7, if developed as a shopping center, would generate approximately 879 more peak hour trips than under the current CDMP designation.

Table C-10
Estimated Peak Hour Trip Generation
By Current CDMP and Requested Use Designations
Truncated Study Area C

Application Number	Assumed Use For Current CDMP Designation/ Estimated No. Of Trips	Assumed Use For Requested CDMP Designation/ Estimated No. Of Trips	Estimated Trip Difference Between Current and Requested CDMP Land Use Designation
6	Open Land/ N/A*	Restricted Industrial & Office - Warehouses (54,450 sq. ft.) / 40	+40
7	Open Land - Single Fam. Resid. (4 Units) / 6	Business & Office - Shopping Center (282,262 sq. ft.) 885	+879

Source: Institute of Transportation Engineers, Trip Generation, 7th Edition, 2003. Includes pass-by trips adjustment factor. Adjustment factor taken from Trip Generation, 7th Edition, 2003.

Note: * The application site is currently designated as "Open Land" and is less than 5 acres in size; therefore, no residential use is permitted on the property.

Transit Service

Existing Service

Metrobus Routes 7, 8, 11, 41, 71, 95, 147, Doral Connection, East-West Connection, Flagler MAX, Sweetwater Circulator and the West Dade Connection, serve Study Area C. Table C-10 shows the existing service frequency in summary form.

Table C-11
Metro Bus Route Service
Truncated Study Area C

Route No.	Weekday Headways*		Proximity in miles to App. No. 6	Proximity in miles to App. No.7	Feeder, Local or Express
	Peak	Off-Peak			
7	30	40	2	4.5	L
8	30	30	3.5	3.25	L/F
11	15	20	3	3.25	L/F
41	30	45	1.25	5	L/F
71	30	30	2	3.25	L
95	5	30	2.5	6.5	E
147	30	60	1	0.5	L
Doral Conn.	30	60	1.75	4.75	L/F
East-West Conn.	30	60	1.5	5	L/F
Flagler MAX	15	30	2	1.75	L/F
Sweetwater Cir.	15	20	2	2.25	L
West Dade Conn.	30	30	2	0.5	L

Source: Miami-Dade Transit Agency, 2005

Notes: F means feeder service to Metrorail

L means local service route

E means express service

*Headway in minutes.

Future Conditions

Transit improvements to the existing transit service in Truncated Study Area C, such as improved headways and extensions to the current routes, are being planned for the next five years as noted in the 2005 Transit Development Program (TDP) and in the People's Transportation Program (PTP). Table C-12 shows service improvements programmed for existing routes within Truncated Study Area C as well as the new routes proposed for the area.

Table C-12
Planned Transit Improvements
Truncated Study Area C

Route	Improvement Description
8	All night service, every 60 minutes, seven days a week. Serves the Government Center station. Extend Westchester short trips to FIU Terminal via SW 16 St. Extend route to FIU on weekends via both 8St and SW 24 St Extend service to Miccosukee resort every 30 minutes.
Route 41	Improve midday from 60 to 30 minutes Improve weekend from 60 to 30 minutes Improve peak headways from 30 to 20 minutes. Improve peak headways from 20 to 15 minutes.
51 Flagler MAX	Introduce weekend service.
71	Improve peak headways from 30 to 15 minutes.
95X	Introduce midday service into the Civic Center. Introduce weekend service.
137 West Dade Connection	Improve peak headways from 30 to 15 minutes.
147	Improve peak headway from 30 to 20 minutes.
147	Improve midday headway from 60 to 30 minutes.
147	Improve peak headway from 20 to 15 minutes.
238 East-West Connection	Improve weekend headways from 60 to 30 minutes. Improve midday headways from 60 to 30 minutes. Improve peak headways from 30 to 20 minutes. Improve peak headways from 20 to 15 minutes
242 Doral Connection	Improve midday headways from 60 to 30 minutes. Improve peak headways from 30 to 15 minutes. Add overnight service seven days a week.

Source: Miami-Dade Transit, August 2005.

There are also two new routes programmed for this area. They are:

New Routes	Improvement Description
FI Turnpike/ SR 836 (SULS)	This premium transit route will be a combination of several express routes: West Kendall to CBD, West Kendall to MIC, West Kendall to CBD via Dolphin Mall/Miami International Mall, and Dolphin Mall/Miami International Mall to the MIC.
Westchester to MIA MAX	New premium service between the SW Westchester area and the Miami International Airport

Source: Miami-Dade Transit, July 2005

The projected transit improvements for Truncated Study Area C are estimated to cost approximately \$1,570,890 in annual operating cost and a one time capital cost of \$2,827,690 for a total cost of \$4,398,580. These costs reflect only the cost of that portion of route improvements within Truncated Study Area C.

Major Transit Projects

Regarding future transit projects within this area, the East-West Corridor Study is currently underway. An evaluation of the previous Final Environmental Impact Statement (FEIS) is being conducted for a rail project along the corridor. The corridor extends along the S.R 836 Expressway, between FIU's Main Campus and the Miami Intermodal Center (MIC).

Application Impacts

For Truncated Study Area C, two application requests were submitted to amend the CDMP (Application Nos. 6 and 7). A trip-generation analysis was performed in the Traffic Analysis Zones (TAZ) where the Applications are located. In TAZ 832, the expected transit impact by Application No. 6 is minimal. If granted, this Application would not warrant changes beyond those already planned for the area.

In TAZ 836, where Application No. 7 is located, 24 additional transit trips would be generated by the Application. Therefore no changes beyond those already planned for the area would be warranted.

Water and Sewer

Water and sewer service is provided to Truncated Study Area C by the Miami-Dade Water and Sewer Department (WASD) and the City of Hialeah.

Potable Water Supply

Most of the unincorporated-area developed portions of Truncated Study Area C are provided with potable water service by the WASD System. Potable water for Truncated Study Area C is treated at WASD's Hialeah-Preston Water Treatment Plant for which the primary source of raw water is the Northwest, Hialeah-Preston and Miami Springs wellfields. These wellfields have a maximum permitted water withdrawal allocation of 235 mgd from the South Florida Water Management District (SFWMD). The plant has a treatment capacity of 225 mgd and had an average daily flow of 158.2 mgd during 2004-2005. The plant currently has approximately 36.4 mgd, or 16.2 percent of its treatment capacity available to meet increased demands.

Sewer Service

Truncated Study Area C is served by WASD's Central District Wastewater Treatment Plant located on Virginia Key. This facility has a permitted flow design capacity of 143.0 mgd and a utilization rate of about 79 percent in 2004-2005. About half of the developed portion of Truncated Study Area C is provided with sanitary sewers, with the rest of the area served by septic tanks.

Water and Sewer Improvements

Concerns regarding sewer overflows during major storm events have resulted in the County entering into a settlement agreement with the Florida Department of Environmental Protection (FDEP) in July 1993, a First Partial Consent Decree with the U.S. Environmental Protection Agency in September 1993, and a Second and Final Partial Consent Decree in April 1994. Under these agreements, the County agreed to implement more than \$1.169 billion in improvements to the wastewater collection and treatment system. Based on projects identified in the proposed 2005-2011 six-year capital improvement program, the Miami-Dade Water and Sewer Department will continue to upgrade the countywide water and wastewater systems, specifically addressing deficiencies that are cited in the Consent Decrees.

Water and Sewer Service to Application Areas

Amendment Application Nos. 6 and 7 are located in Truncated Study Area C. The locations of the most proximate water and sewer connections to these sites are detailed in Table C-13. The effect of these amendments on water and sewer demand is specified in Table C-14.

Table C-13
Available Water and Sewer Connections for Applications in Truncated Study Area C

	Application No.	Distance to Main	Diameter of Main (inches)	Location of Main	Utility (1)
WATER	6	900 feet	12	NW 25 Street	WASD
	7	900 feet	30	SW 137 Avenue and SW 8 Street	WASD
SEWER	6	810 feet	8F	NW 25 Street and NW 117 Avenue	WASD
	7	900 feet	24F	SW 137 Avenue and SW 8 Street	WASD

(1) Utility Serving Application Area

WASD = Miami-Dade Water and Sewer Department

(G = Gravity Main; F = Force Main)

Source: Department of Environmental Resources Management, 2005

Miami-Dade Water and Sewer Department, 2005

Application No. 6. Water service to the site of Application No. 6 would require connection to a 12-inch water main approximately 900 feet from the site that runs along NW 25 Street, between NW 117 Avenue and NW 127 Avenue, according to the Water and Sewer Department. Any proposed water main extension inside the developer's property shall be 12 inches minimum with two points of connection. There is a recently installed 8-inch gravity sewer main approximately 810 feet north of the site from which the developer can connect. Any proposed sewer extension inside the developer's property shall be 8 inches minimum.

Application No. 7. Water service to the site of Application No. 7 would require connection to a 30-inch water main approximately 900 feet from the site, at SW 137 Avenue and SW 8 Street. Any proposed water main extension inside the developer's property shall be 12 inches minimum with two points of connection. There is an existing 24-inch force sewer main approximately 900 feet north of the site from which the developer can connect. Any proposed sewer extension inside the developer's property shall be 10 inches and 8 inches minimum. This point of connection can be changed at the time the developer enters into an agreement with WASD. A public pump station will also be required as part of the agreement.

Table C-14
Water and Sewer Demand for Applications in Truncated Study Area C
(in gallons per day - GPD)

Application No.	Water and Sewer Demand (GPD) (1)	Change From Current Designation (GPD) (1)
6	5,445	+5,445
7	47,045	+45,645

(1): Highest demand possible under existing and proposed CDMP designations

Source: Miami-Dade Department of Environmental Resources Management, 2005

Miami-Dade Department of Planning and Zoning, 2005

WASD's regional wastewater treatment and disposal facilities have limited available capacity. Consequently, approval of development orders which will generate additional wastewater flows are being evaluated by DERM on a case-by-case basis. Approvals are only granted if the application for any proposed development order is certified by DERM so as to be in compliance with the provisions and requirements of the Settlement Agreement between Miami-Dade County and the State of Florida Department of Environmental Protection and also with the provisions of the Environmental Protection Agency consent decree.

Furthermore, in light of the fact that the County's sanitary sewer system has limited sewer collection/transmission and treatment capacity, no new sewer service connections can be permitted until adequate capacity becomes available. Consequently, final development orders for new construction may not be granted unless adequate capacity or alternative means of sewage disposal can be obtained. Use of an alternative means of sewage disposal shall be an interim measure, with connection to the public sanitary sewer system required upon availability of adequate collection/transmission and treatment capacity.

At the present time, the potable water supply system meets the Level of Service standards as established in Policy 2A of the Water, Sewer and Solid Waste Element of the Miami-Dade County Comprehensive Development Master Plan.

Solid Waste

Since the Department of Solid Waste Management (DSWM) assesses capacity system-wide based, in part, on existing waste delivery commitments from both the private and public sectors, it is not possible to make determinations concerning the adequacy of solid waste disposal facilities relative to each individual application. Instead, the DSWM issues a periodic assessment of the County's status in terms of 'concurrency' – that is, the ability to maintain a minimum of five years of waste disposal capacity system-wide. The County is committed to maintaining this level in compliance with Chapter 163, Part II, F.S., and currently exceeds that standard by nearly four (4) years (See Solid Waste section in Chapter 2 of this report).

Application Nos. 6 and 7 lie outside of the 2005 Urban Development Boundary (UDB), but within the DSWM's waste service area for garbage and trash collections. The closest DSWM facility for Application Nos. 6 and 7 is the Resources Recovery Facility (6990 NW 97th Avenue), located approximately 9 and 15 miles away, respectively. Under the DSWM's current policy, only residential customers paying the annual waste collection fee and/or the Trash and Recycling Center fee are allowed the use of this type of facility. Due to the character of the requests, however, there are no impacts on collection services. The impact on the disposal and transfer facilities would be the incremental and cumulative costs of providing disposal capacity for DSWM Collections, private haulers, and municipalities, which are paid for by the users. The DSWM is capable of providing such disposal service for each of the applications.

Fire and Rescue Service

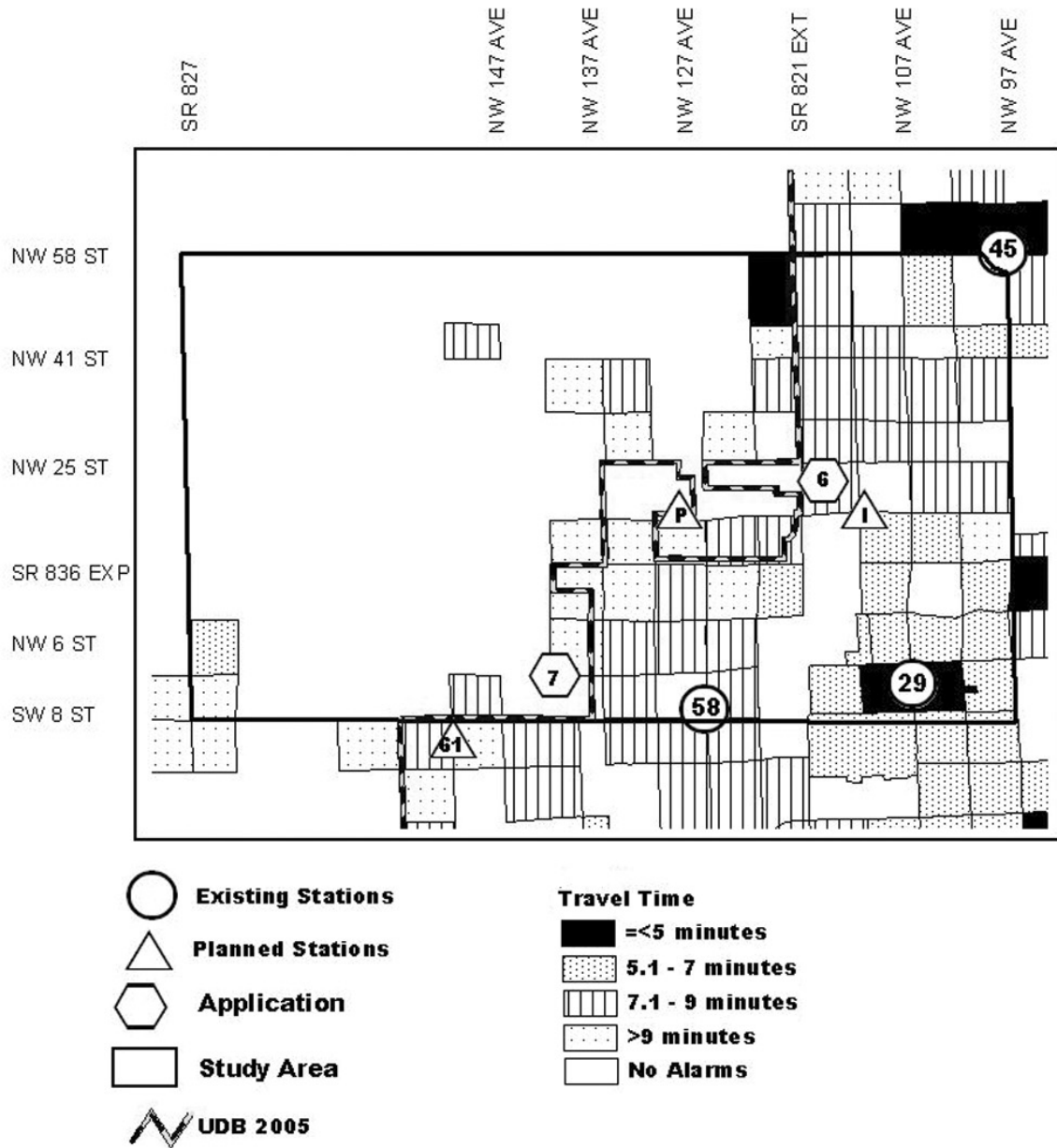
The area surrounding the application sites is currently served by Miami-Dade Fire Rescue Stations 29, 45, and 58. The Dolphin Station (I) is scheduled for construction in fiscal year 2007-2008 in the vicinity of NW 112 Avenue and NW 17 Street. The planned station is poised to mitigate impacts to existing services resulting from the proposed amendment applications. (See Figure C-15).

Travel Times

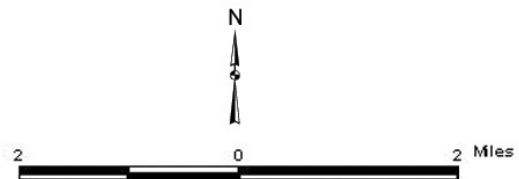
Average travel time to alarms at the location of Application No. 6 is approximately 6.75 minutes. Travel time for Life Threatening Emergencies is approximately 7.30 minutes and 6.20 minutes for Structure fires. The current CDMP designation (Open Land) generates a total of 0 annual alarms. The proposed CDMP designation (Restricted Industrial and Office) will allow a proposed potential development totaling 54,450 sq. ft., which is anticipated to generate 3 annual alarms. This will result in a minimal impact to existing fire rescue services.

Average travel time to alarms at the location of Application No. 7 is approximately 8.20 minutes. Travel time for Life Threatening Emergencies is approximately 9.00 minutes and 7.40 minutes for Structure fires. The current CDMP designation (Open Land) generates a total of 2 annual alarms. The proposed CDMP designation (Business and Office) will allow a proposed potential development totaling 470,448 sq. ft., which is anticipated to generate 75 annual alarms. This will result in a severe impact to existing fire rescue services.

Figure C-15
FIRE AND RESCUE: APPLICATION NOS. 6 & 7



SOURCE: MIAMI-DADE COUNTY DEPARTMENT
OF PLANNING AND ZONING, AND
MIAMI-DADE FIRE DEPARTMENT, 2005



Fire Flow

The required fire flow for both application sites is 3,000 gallons per minute (gpm). There is an existing 12" water main along NW 25 Street that could potentially service Application No 6. Application No. 7 could be potentially serviced by is an existing 30" water main at the intersection of SW 8 Street and SW 137 Avenue. Available fire data shows that adequate flow is available to Application No. 7. According to the Miami-Dade Fire Department, available fire flow should be adequate for Application No. 6 if new water mains and hydrants are installed per Article XIVA, Water Supply for Fire Suppression, of the Miami-Dade County Code.

County Parks

County-owned park and recreational facilities serving Truncated Study Area C are shown on Figure C-16. These parks are described on Table C-15, which lists the name and acreage for each park site. The nearest park site to Application No. 6 is North Trail Park, a 15-acre Community Park located at NW 8 Street and NW 127 Avenue. The nearest park site to Application No. 7 is Tamiami Trail Park, a 2-acre Neighborhood Park located at SW 6 Street and SW 127 Avenue.

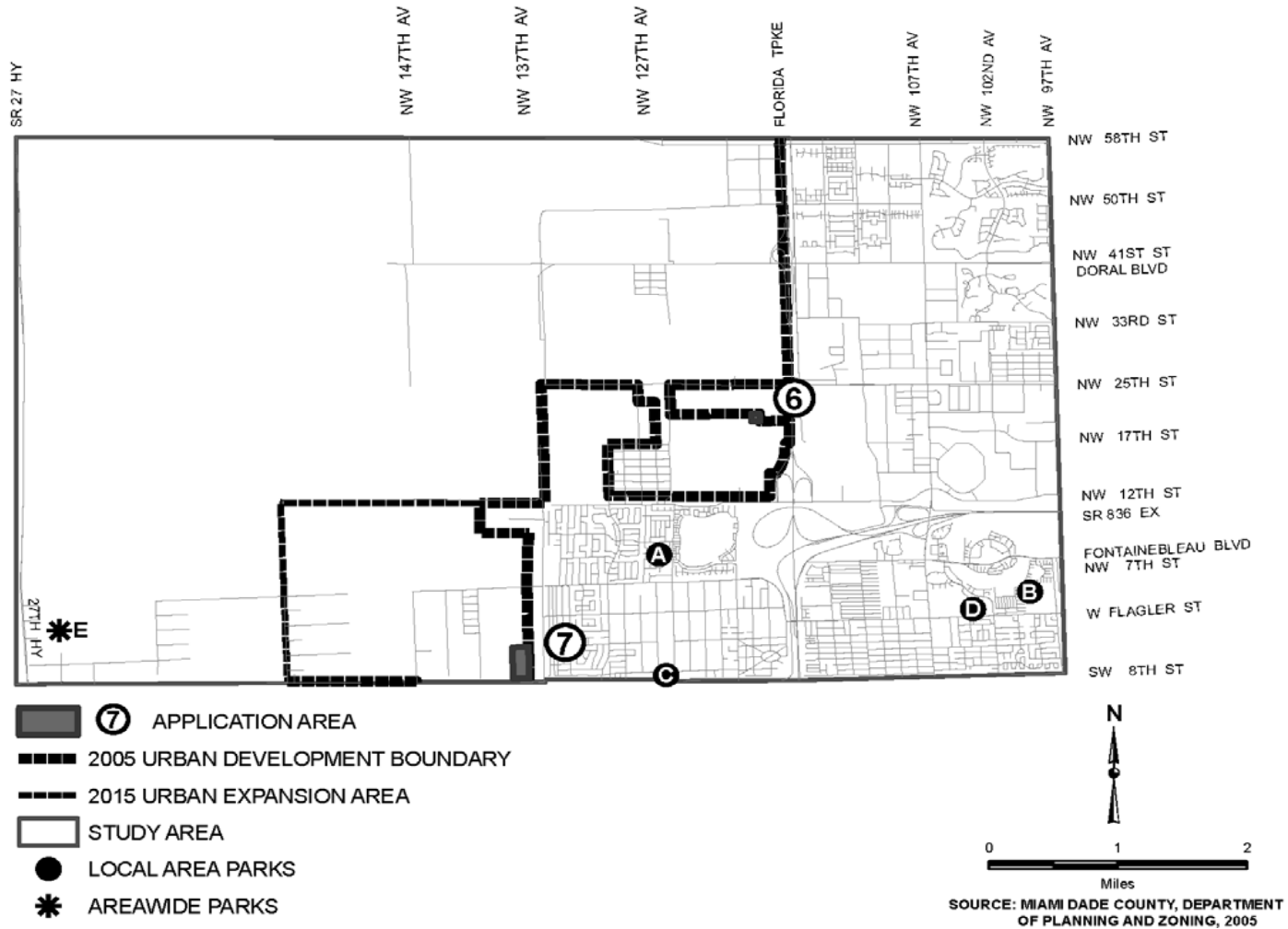
Table C-15
County Park and Recreation Open Space Facilities: Truncated Study Area C

Park Identifier (Figure C-16)	Name of Park	Park Classification	Acreage
A	North Trail	Community	15
B	Ruben Dario	Community	15
C	Tamiami Trail	Neighborhood	2
D	The Womens Park	Special Purpose	15
E	Trail Glades	Special Activity	675

Source: Miami-Dade County Park and Recreation Department, 2005.

Study Area C is located in Park Benefit District (PBD) 1, which has a surplus capacity of 544.79 acres when measured by the County concurrency level-of-services standard. Application Nos. 6 and 7 will not generate any residential population and therefore not impact the level of service.

Figure C-16
COUNTY PARKS: APPLICATION NOS. 6 & 7



Public Schools

Table C-16 lists the mainstream public schools in the mapped portion of Truncated Study Area C, indicating school name and type, October 2004 enrollment, the Florida Inventory of School Houses (FISH) Design Capacity which includes permanent and relocatable student stations, and FISH percent. The locations of these schools are identified on Figure C-17. As can be seen, elementary schools in Truncated Study Area C had an October 2004 enrollment of 4,259 a FISH Design Capacity of 3,483 and a FISH percent of 122%. Middle schools had an October 2004 enrollment of 2,581, a FISH Design Capacity of 2,438, and a FISH percent of 106%. There are no senior High Schools located in the study area. The total October 2004 enrollment for Truncated Study Area C is 6,840 with a FISH Design Capacity of 5,921 and a FISH percent of 115%.

Due to the non-residential nature of the proposed amendments, Application No. 6 and 7 will have no effect on public school enrollment or capacity. A complete listing of comments from the Miami-Dade Public Schools is attached as Appendix A.

Table C-16
2004 Public School FISH Rates:
Truncated Study Area C

School Identifier (Figure C-17)	Name of School	October 2004 Membership	FISH Design Capacity	FISH Percentage
ELEMENTARY SCHOOLS				
A	E.W.F. Stirrup	975	916	106
B	John I. Smith (1)	1,276	901	142
C	Marjory S. Douglas (2)	1,061	917	116
D	Sweetwater	947	749	126
TOTAL ELEMENTARY		4,259	3,483	122
MIDDLE SCHOOLS				
E	Paul W. Bell	1,540	1,175	131
F	Ruben Dario	1,041	1,263	82
TOTAL MIDDLE		2,581	2,438	106
		6,840	5,921	115
STUDY AREA TOTAL				

(1) Includes Primary Learning Center "A1"

(2) Includes Primary Learning Center "B"

Source: Miami-Dade County Department of Planning and Zoning, 2005
Miami-Dade County Public Schools, 2004

Figure C-17
PUBLIC SCHOOLS: APPLICATION NOS. 6 & 7

